

ORDINANCE NO. ____

PROPOSED ORDINANCE NO. 25-050

AN ORDINANCE RELATING TO ZONING; MAKING FINDINGS; AMENDING ORDINANCE 4773, AS AMENDED, TO PROVIDE FOR A MAJOR MODIFICATION OF PLANNED UNIT DEVELOPMENT (PUD) ZONING TO ALLOW 36 SINGLE-FAMILY ATTACHED (TOWNHOME) DWELLING UNITS ON APPROXIMATELY 8.48 ACRES, ALLOW A CLUBHOUSE/AMENITY CENTER ON APPROXIMATELY 2.48 ACRES, INCREASE THE MAXIMUM NUMBER OF SINGLE-FAMILY DETACHED DWELLING UNITS FROM 14 TO 17 ON APPROXIMATELY 19.02 ACRES, ADOPT A NEW SITE DEVELOPMENT PLAN AND REDUCE THE MAXIMUM NUMBER OF SINGLE-FAMILY DETACHED DWELLING UNITS FROM 46 TO 44 ON APPROXIMATELY 33.24 ACRES AND ADOPT A NEW SITE PLAN TO ALLOW 116 SINGLE-FAMILY ATTACHED (TOWNHOME) DWELLING UNITS AND 700 MULTI-FAMILY DWELLING UNITS ON APPROXIMATELY 57.74 ACRES OF PROPERTY GENERALLY LOCATED NORTH OF INTERSTATE-4, SOUTH OF HEATHERPOINT DRIVE, EAST AND WEST OF CARPENTERS WAY AND SOUTH OF WEDGEWOOD ESTATES BOULEVARD; PROVIDING CONDITIONS; FINDING CONFORMITY WITH THE COMPREHENSIVE PLAN; PROVIDING FOR SEVERABILITY; PROVIDING AN EFFECTIVE DATE.

WHEREAS, the Planning and Zoning Board held a public hearing on September 16, 2025 to consider the request of Chloe Dyal and Chirag Kikani, Tract Engineering, on behalf of owner SJD Development, LLC, to amend Ordinance 4773, as amended, to modify Planned Unit Development (“PUD”) zoning to allow 36 single-family attached (townhome) dwelling units on approximately 8.48 acres (Tract 9B within Area W), remove a portion of Area U and create a new Tract 12 in Area W for a clubhouse/amenity center to serve future multi-family residential uses on Tracts 4 and 11, in lieu of a previously approved 70,100 sq. ft. assisted living facility on approximately 2.48 acres (Area U), increase the maximum number of single-family detached dwelling units from 14 to 17 on approximately 19.02 acres (Tract 1 within

Area W), adopt a new site development plan and reduce the maximum number of single-family detached dwelling units from 46 to 44 on approximately 33.24 acres (Tracts 5, 6 and 7 within Area W) and adopt a new site plan to allow 116 single-family attached (townhome) dwelling units and 700 multi-family dwelling units, in lieu of previously approved entitlements for 834 multi-family dwelling units on approximately 57.74 acres (Tracts 2, 3, 4, 10 and 11 within Area W), generally located north of Interstate-4, south of Heatherpoint Drive, east and west of Carpenters Way and south of Wedgewood Estates Boulevard, as more particularly described on Exhibit “W-1” and graphically depicted on Exhibit “W-2,” (the “Property”); and

WHEREAS, the Planning and Zoning Board approved the requested PUD zoning modification on November 18, 2025 and recommended said modification to the City Commission; and

WHEREAS, the City Commission, after due public notice and a public hearing at which all interested persons were afforded the opportunity to be heard, finds that the Property should be classified or zoned as recommended by the Planning and Zoning Board;

NOW, THEREFORE, BE IT ORDAINED BY THE CITY COMMISSION OF THE CITY OF LAKELAND, FLORIDA:

SECTION 1. The foregoing findings are true and correct and are hereby adopted and made a part hereof.

SECTION 2. The PUD zoning for the Property is hereby modified, subject to the following amended conditions:

Area U and W in Ordinance 4773, as amended

XXII. Area U

- A. Land Use Intensity: PUD-49
- B. Permitted Uses: Assisted living facility in one or more buildings.
- C. Maximum Floor Area:
Phase I: 21,100 square feet.
~~Phase II: 70,100 square feet~~
- D. Maximum Building Height:
Phase 1: One (1) story
~~Phase 2: Two (2) stories~~
- E. Setbacks
 - 1. From Carpenters Way: Fifty (50) feet.
 - 2. From all other project boundaries; Twenty-five (25) feet.
- F. Site Development Plan:
The site shall be developed in accordance with the site development plan included as Exhibit "U-1"
- G. Requirements for parking, landscaping, buffering etc. shall be in accordance with City of Lakeland Land Development Regulations. This shall include at a minimum the installation of a hedge and canopy trees on the ratio of one tree per fifty (50) linear feet along Carpenters Way.

XIV. Area W

- A. Permitted Uses.
 - 1. Single-Family Detached Residential Uses: Tracts 1, 5, ~~6~~, and 7
 - ~~2. Single-Family Attached Residential Uses: Tracts 2*, 4, 8, 9, and 10 and 11**~~
 - 2. Single-Family Attached Residential Uses: Tracts 8, 9B, and 10
 - ~~3. Multi-Family Residential Uses: Tracts 2*, 3, 4, 10 and 11**~~
 - 3. Multi-Family Residential Uses: Tracts 2, 3, 4 and 11
 - ~~4. Future Development Tract: Tract 9~~

~~*As an alternative development plan in lieu of 230 multi family dwelling units, Tract 2 may be developed for single family attached dwelling units in accordance with the maximum intensity of use~~

~~specified in Condition B. 2. and Exhibit "W 3a." Prior to the issuance of a building permit, written notice shall be provided to the Community and Economic Development Department in which the developer elects to develop Tract 2 for one of the two approved development plans. When the written notice of election has been delivered, the development plan that was not chosen will no longer be an approved use for the property.~~

~~**As an alternative development plan in lieu of 330 240 multi-family dwelling units, Tracts 4, 10 and 11 may be developed for single-family attached dwelling units in accordance with the maximum intensity of use specified in Condition B. 2. and Exhibit "W 3b." Prior to the issuance of a building permit, written notice shall be provided to the Community and Economic Development Department in which the developer elects to develop Buildings 4D, 4E, 4F and 4G and Tracts 10 and 11 for one of the two approved development plans. When the written notice of election has been delivered, the development plan that was not chosen will no longer be an approved use for the property.~~

4. Amenity Center for Multi-Family Residential Uses: Tract 12

5. Right-of-way Donations: Tract 6 and 9A

B. Maximum Intensity of Use: The maximum intensity of use for each development tract shall be as follows.

1. Single-Family Detached Residential Uses (Tracts 1, 5, 6 and 7):

a. ~~Tract 1: 44-17~~ single-family detached dwelling units

b. ~~Tract 5: 44-15~~ single-family detached dwelling units

c. ~~Tract 6: 12~~ single-family detached dwelling units

d. ~~Tract 7: 20~~ single-family detached dwelling units

c. Tract 7: 29 single-family detached dwelling units

2. Single-Family Attached Residential Uses (Tracts 2, 4, 8, 9B, and 10 and 14)

a. ~~Tract 2: 96~~ single-family attached dwelling units

b. ~~Tract 8: 60~~ single-family attached dwelling units

c. ~~Tracts 4, 10 and 11: 162~~ single-family attached dwelling units

a. Tract 8: 60 single-family attached dwelling units

b. Tract 9B: 36 single-family attached dwelling units

c. Tract 10: 116 single-family attached dwelling units

3. Multi-family Residential Uses (Tracts 2, 3, 4, ~~10~~ and 11)

- a. Tract 2: ~~230-240~~ multi-family dwelling units
- b. Tract 3: ~~454-60~~ multi-family dwelling units
- ~~c. Tracts 4/10/11: 450 multi-family dwelling units~~
- c. Tract 4: 280 multi-family dwelling units
- d. Tract 11: 120 multi-family dwelling units

4. Transfer of Units Between Development Tracts: Residential units may be transferred between development tracts with the same residential use type (i.e. multi-family residential to multi-family residential or single-family detached residential to single-family detached residential). A maximum five percent (5%) increase in residential units may be permitted above the amount approved in any receiving tract provided that the total cumulative residential density for the development does not exceed 973 dwelling units. Any redistribution of units within this limit shall not require a formal amendment to the approved PUD or Development Agreement but will be subject to staff-level review and approval for consistency with the overall master plan and available infrastructure capacity.

C. Development Standards:

1. Single-Family Detached Residential Uses:

- a. Tract 1: In accordance with the RA-3/SNH context sub-district except as follows
 - i. Minimum Lot Width: 90 feet
 - ii. Minimum Lot Area: 10,000 sq. ft.
 - iii. Minimum Front Setback: 25 feet
 - iv. Minimum Street Side Setback: 15 feet
 - v. Minimum Side Setback: 7.5 feet
 - vi. Minimum Rear Yard Setback: 25 feet
- b. Tracts 5, ~~6~~ and 7: In accordance with the RA-3/SNH context sub-district except as follows
 - i. Minimum Lot Width: 60 feet
 - ii. Minimum Lot Area: 7,500 sq. ft.
 - iii. Minimum Front Setback: 25 feet
 - iv. Minimum Street Side Setback: 15 feet
 - v. Minimum Side Setback: 7.5 feet

- vi. Minimum Rear Yard Setback: 25 feet
2. Single-Family Attached Residential Uses: In accordance with the MF-12/SNH context sub-district except as follows.
- a. Minimum Setbacks for Townhouse Building Groups
 - i. From Property Lines of Adjacent Multi-Family Uses: 25 feet.
 - ii. Minimum Internal Setback Between Building Groups: 10 feet.
 - b. Development Standards for Single-Family Attached Dwellings:
 - i. Minimum Lot Width: 19 feet
 - ii. Minimum Living Area: 1,463 sq. ft.
 - iii. Minimum Front Setback: ~~25~~20 feet
 - iv. Minimum Street Side Setback: 15 feet
 - v. Minimum Interior Side Setback: 0 feet
 - vi. Minimum Rear Yard Setback: 10 feet
 - vii. Maximum Building Height: Two (2) stories.
 - viii. Maximum number of dwellings per group: Six (6)
 - c. Buffering for Single-Family Attached Dwellings Located South of Fairfield on the Tee: An enhanced buffer is required between existing townhomes in Fairfield on the Tee and new single-family attached dwellings located in Tract 10. To provide a minimum separation distance of 40 feet between structures, and 20 feet from rear lot lines to rear lot lines, a Type "B" landscape buffer as shown in Exhibit W-4b, with a minimum width of 20 feet, shall be installed along the southern property line of Fairfield on the Tee.
3. Multi-Family Residential Uses: In accordance with the MF-12/SNH context sub-district except as follows.
- a. Minimum Setbacks:
 - i. From Property Lines of Adjacent Multi-Family Uses: 25 feet.
 - ii. Minimum Internal Setback Between Buildings: 15 feet
 - b. Maximum Building Height: ~~Four (4)~~ Three (3) stories**
 - c. Architectural Design: At least 75% of the units above the first floor of each building shall have a balcony. Multi-family buildings shall reference the architectural elements of

existing multi-family uses located within the Carpenters Way PUD, including but not limited to, roof type, finishing materials, and paint colors used.

**Four-story buildings shall be limited to the specific buildings listed in the Site Data Table and the locations shown on the Master Site Plan, Exhibit "W-3," and the Site Data Table & Phasing Plan, Exhibit "W-7." All other multi-family buildings shall be limited to a maximum height of either two (2) or three (3) stories as specified in Exhibit "W-7".

- D. Site Development Plan: The site shall be developed in accordance with the proposed site development plan included as Exhibits "W-3", "W-3a" and "W-3b" and the site data table and phasing plan included as Exhibit "W-7." With the approval of the Director of Community & Economic Development, minor alterations may be made at the time of site plan review without requiring a modification to the PUD zoning.
- E. Landscaping and Buffering: In accordance with Section 4.5 of the Land Development Code, the Landscape Buffer Plan, Exhibit "W-4a" and Typical Buffer Details, Exhibit "W-4b." In addition to the buffering shown on the Landscape Buffer Plan, a Type "D" buffer as shown in Exhibit "W-4b" shall be installed and maintained adjacent to the north and west boundaries of the United Brotherhood of Carpenters and Joiners of America cemetery. Within Tract 8, the buffer shall include a six-foot tall vinyl privacy fence. Within Tract 2, the buffer shall include either a black or green vinyl chain link fence or decorative metal picket fencing with a minimum height of four feet.
- F. Access
 - 1. Access from the development tracts onto Carpenter's Way shall be limited to the intersections illustrated in Exhibit "W-3"
 - 2. Access improvements on Carpenters Way at street or driveway connections to the development tracts, such as turning lanes, shall be constructed in accordance with the standards and specifications of the Florida Department of Transportation and City as appropriate and determined during the permitting process. The cost of such improvements shall be the responsibility of the applicant.
 - 3. All roads within the development shall be constructed in accordance with City of Lakeland standards.
 - 4. Access to the United Brotherhood of Carpenters and Joiners of America cemetery shall be formalized through a recorded agreement. If such agreement already exists, a copy of the agreement shall be provided prior to the issuance of the first building permit for the development.
- G. Civic Open Space and Recreational Amenities

1. A development-wide paved multi-use trail system shall be constructed as illustrated in Exhibits “W-5b,” “W-6a,” “W-6b,” “W-6c,” and “W-6d.” “W-6e,” and “W-6f.”
 2. Tracts 2 and 3 shall have their own amenity center with a clubhouse, swimming pool and other recreational facilities.
 3. Tracts 4, 10 and 11 shall have a master amenity center consisting of a clubhouse, swimming pool and other recreational facilities that is shared by the residents of all three tracts. This master amenity center shall be located on Tract 12.
- H. Off-street Parking: Off-street parking shall be provided in accordance with Section 4.11 and Table 4.11-1 of the Land Development Code. If the minimum amount of parking cannot be provided for a specific development tract, the maximum number of multi-family dwelling units shall be reduced to align with amount of parking provided.
- I. Signage: In accordance with Section 4.9 of the Land Development Code.
- J. Outdoor Lighting: Except for City-approved streetlights within rights-of-way, all outdoor lighting fixtures shall be of the “fully shielded” type such that the light emitting, distributing, reflecting, and refracting components of the light fixture (lamp, lens, reflective surfaces, etc.) shall not extend beyond the opaque housing of the fixture.
- K. Stormwater Ponds: stormwater ponds shall be designed as water features with a natural appearance and shape. The use of fencing shall be minimized, but in the event fencing is required, fence materials used shall consist of either black or green vinyl chain link or decorative metal picket fencing. Any landscaping shall be located on the outside of such fencing.
- L. Transportation:
1. Prior to the first site plan or preliminary plat submittal, the developer and City shall execute and record in the Official Records of the Polk County Clerk of Courts a Development Agreement through which the timing of the following specific transportation concurrency mitigation requirements are addressed. This agreement shall be executed by the City Commission and recorded with the Polk County Clerk of Courts.
 - a. Right-of-way dedication to the City of Lakeland to accommodate the Lakeland Park Center Drive Extension south of Carpenters Way. The dedicated right-of-way and design shall minimize impacts to an existing gas transmission line and accommodate necessary stormwater treatment facilities for this future roadway or shared facilities for the roadway and adjacent development.

- b. Right-of-way dedication and construction of a roundabout or traffic signal with turn lanes at the Carpenters Way/Wedgewood Estates Boulevard/Heatherpoint Drive intersection, including the realignment of Wedgewood Estates Boulevard to align with Heatherpoint Drive as generally illustrated in Exhibit "W-5a".
 - c. Installation of a transit shelter and bus bay shall be installed on Lakeland Park Drive at the entrance to Tracts 3A/3B to serve a re-alignment of the Citrus Connection's Pink Line, which is anticipated to occur once the apartment development phases commence.
- 2. A major update to the Wedgewood Redevelopment Traffic Impact Study (dated March 2022) shall be conducted for all remaining project phases for which a building permit has not been requested by ~~September 1, 2027~~ November 1, 2035.
- 3. Bicycle/Pedestrian Infrastructure
 - a. An integrated multi-use trail network shall be implemented throughout the proposed development area as depicted in Exhibits "W-5a" and "W-5b". The appropriate cross-sections for each trail and street type shall be included with site plan and subdivision construction plan submittals for the applicable project phases. Raised crosswalks shall be installed at each location where this trail network crosses Carpenters Way.
 - b. Concurrent with the construction of the Carpenters Way/Wedgewood Estates Boulevard-Heatherpoint Drive ~~roundabout intersection~~ realignment, a sidewalk shall be constructed along the south side of Carpenters Way to the eastern terminus of the existing frontage sidewalk at Douglas Cook Park.
 - c. ADA-compliant sidewalk routes shall be constructed between each principal multi-family residential building and amenity to the planned trail network and existing frontage sidewalks on the adjacent street network.
 - d. Bicycle parking for each multi-family residential building and amenity shall be provided in compliance with Section 4.11.6 of the Land Development Code, with designs in compliance with Index 900 of the City Engineering Standards Manual.
- 4. Prior to the first Certificate of Occupancy for Tract 2, a roadway "stub-out" shall be constructed to the eastern site boundary in support of a future roadway connection to Arteva Drive.

- 5. Current and future developers within this area of the PUD shall work with the City to support its efforts to locate a high-speed rail (such as Brightline) stop in its vicinity.
- M. Maintenance of Trails, Stormwater Infrastructure and Common Areas: Each development tract shall have a Homeowners Association, Property Owners Association, or some other legal entity which shall be responsible for the perpetual maintenance of trails and common areas within the boundaries of the development tract.

SECTION 3. The City Commission does hereby expressly find that the provisions of this Ordinance are in conformity with the Comprehensive Plan of the City of Lakeland adopted by Ordinance 5885.

SECTION 4. All ordinances or parts of ordinances in conflict herewith are hereby repealed.

SECTION 5. If any word, sentence, clause, phrase, or provision of this Ordinance, for any reason, is held to be unconstitutional, void, or invalid, the validity of the remainder of this Ordinance shall not be affected thereby.

SECTION 6. This Ordinance shall take effect immediately upon the effective date of Small-Scale Amendment #LUS25-002 to the Future Land Use Map, as provided in Ordinance No. _____.

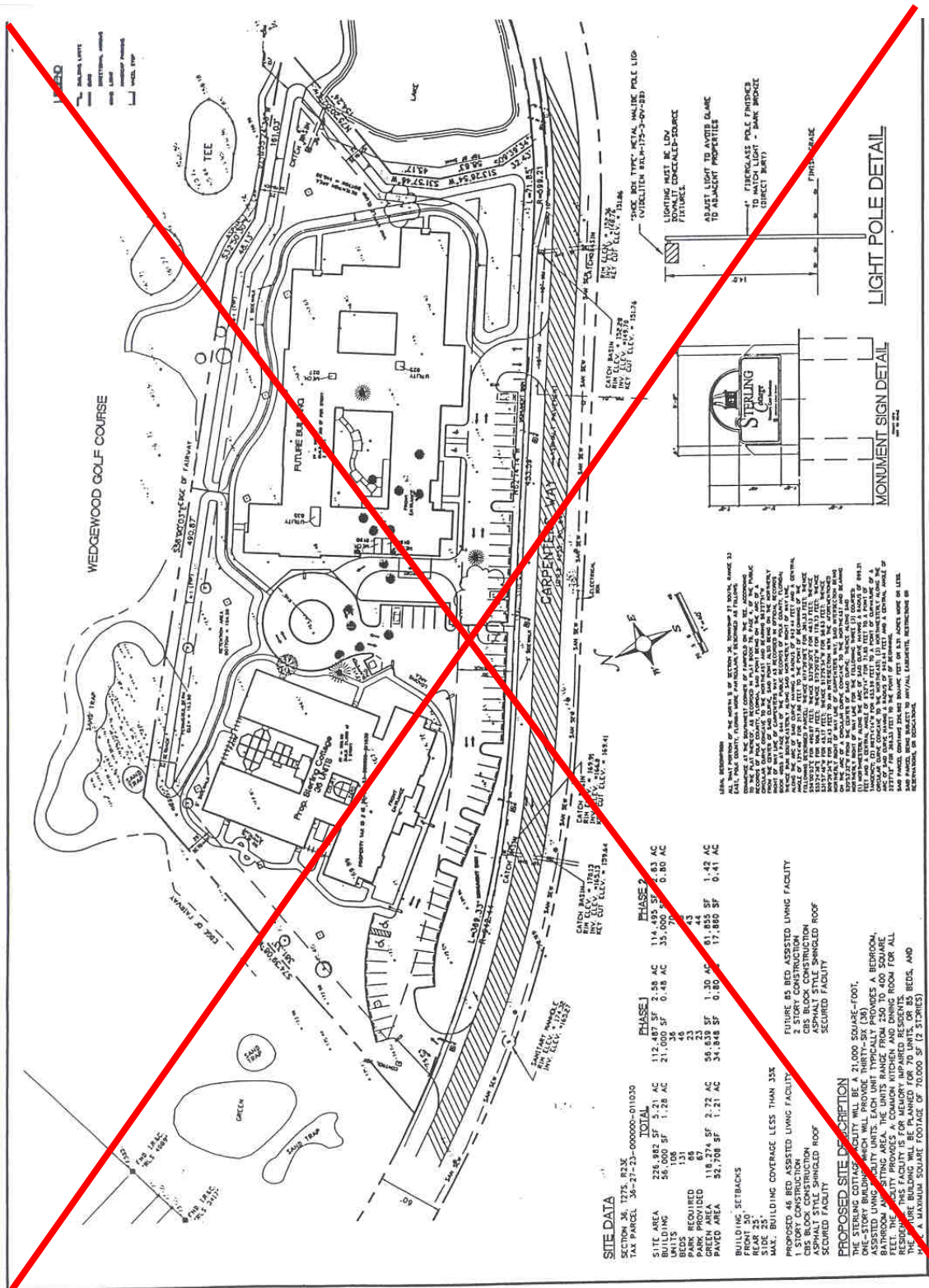
PASSED AND CERTIFIED AS TO PASSAGE this 15th day of December, A.D. 2025.

CITY COMMISSION OF THE CITY OF LAKELAND, FLORIDA

By: _____
H. WILLIAM MUTZ, MAYOR

ATTEST: _____
KELLY S. KOOS, CITY CLERK

APPROVED AS TO FORM AND CORRECTNESS: _____
PALMER C. DAVIS
CITY ATTORNEY



SITE DATA

SECTION 36, T12S, R12E
 TAX PARCEL 36-27-23-000000-01030

	PHASE 1	PHASE 2	TOTAL
SITE AREA	112,487 SF 2.58 AC	114,495 SF 2.63 AC	226,982 SF 5.21 AC
UNIT FLOOR AREA	21,000 SF 0.48 AC	35,700 SF 0.80 AC	56,700 SF 1.28 AC
BEDS	46	70	116
BATHS	131	131	262
PARK PROVIDED	67	44	111
GREEN AREA	118,274 SF 2.72 AC	59,639 SF 1.34 AC	177,913 SF 4.06 AC
PAVED AREA	52,708 SF 1.21 AC	34,848 SF 0.79 AC	87,556 SF 1.99 AC

PROPOSED SITE DESCRIPTION

THE STERLING COMMUNITY WILL BE A 21,000 SQUARE-FOOT, ONE-STORY BUILDING WHICH WILL PROVIDE THIRTY-ONE (31) UNITS, INCLUDING A BEDROOM, BATHROOM AND BATHING AREA. THE UNITS RANGE FROM 250 TO 400 SQUARE FEET. THE COMMUNITY PROVIDES A COMMON KITCHEN AND DINING ROOM FOR ALL UNITS. THE BUILDING WILL BE PLANNED FOR 70 UNITS, OR 83 BEDS, AND WILL HAVE A MAXIMUM SQUARE FOOTAGE OF 70,000 SF (2 STORIES).

BUILDING SETBACKS

FRONT 30'
 SIDE 25'
 MAX. BUILDING COVERAGE LESS THAN 35%

PROPOSED 46 BED ASSISTED LIVING FACILITY

STORY CONSTRUCTION
 CONCRETE FOUNDATION
 ASPHALT STYLE SHINGLED ROOF
 SECURED FACILITY

FUTURE 85 BED ASSISTED LIVING FACILITY

STORY CONSTRUCTION
 CONCRETE FOUNDATION
 ASPHALT STYLE SHINGLED ROOF
 SECURED FACILITY

LEGEND

- EXISTING LIGHTS
- NEW LIGHTS
- EXISTING SIGNAGE
- NEW SIGNAGE
- EXISTING DRIVE
- NEW DRIVE
- EXISTING SIDEWALK
- NEW SIDEWALK

LIBRARY DESCRIPTION:
 THE STERLING COMMUNITY WILL BE A 21,000 SQUARE-FOOT, ONE-STORY BUILDING WHICH WILL PROVIDE THIRTY-ONE (31) UNITS, INCLUDING A BEDROOM, BATHROOM AND BATHING AREA. THE UNITS RANGE FROM 250 TO 400 SQUARE FEET. THE COMMUNITY PROVIDES A COMMON KITCHEN AND DINING ROOM FOR ALL UNITS. THE BUILDING WILL BE PLANNED FOR 70 UNITS, OR 83 BEDS, AND WILL HAVE A MAXIMUM SQUARE FOOTAGE OF 70,000 SF (2 STORIES).

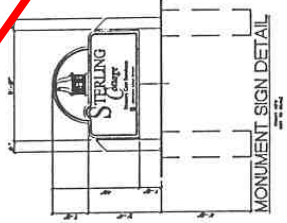
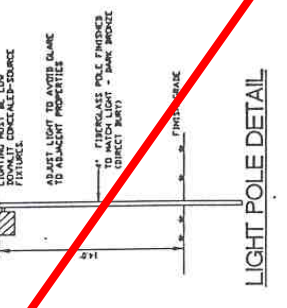


Exhibit "W-1"

Legal Description

Tract 1

Beginning at a point whose Northing is 1369644.334 and whose Easting is 669323.451; thence bearing N 41-3-13.864 E a distance of 293.913; thence bearing N 5-5-2.201 W a distance of 123.838; thence bearing N 89-54-40.000 E a distance of 585.110; thence bearing S 0-30-16.244 E a distance of 897.286; thence bearing S 89-34-30.900 W a distance of 109.474; thence bearing N 27-10-11.222 W a distance of 344.529; thence bearing S 67-52-1.045 W a distance of 837.897; thence bearing S 75-46-40.726 W a distance of 504.832; thence bearing S 89-44-25.447 W a distance of 280.573; thence along a curve to the LEFT, having a radius of 282.593 a delta angle of 28° 24' 19.47", and whose long chord bears N 66-33-54.838 W a distance of 138.670; thence along a curve to the RIGHT, having a radius of 56.266 a delta angle of 63° 06' 44.52", and whose long chord bears N 50-2-14.386 W a distance of 58.892; thence bearing N 20-32-46.160 W a distance of 152.326; thence bearing N 69-41-52.000 E a distance of 76.000; thence bearing S 88-22-19.000 E a distance of 178.650; thence bearing N 88-22-46.662 E a distance of 190.685; thence bearing N 77-59-56.004 E a distance of 195.156; thence bearing N 70-22-13.996 E a distance of 277.616; thence bearing N 76-35-39.621 E a distance of 565.596; thence bearing N 48-22-54.083 W a distance of 239.952 to the point of beginning.

Tract 2

Beginning at a point whose Northing is 1367915.746 and whose Easting is 669607.933; thence bearing N 5-13-46.479 W a distance of 377.370; thence bearing N 54-13-46.919 E a distance of 61.645; thence bearing N 4-10-46.086 W a distance of 266.321; thence bearing S 55-48-6.657 W a distance of 263.144; thence bearing N 84-40-35.152 W a distance of 198.089; thence bearing S 74-13-52.034 W a distance of 339.880; thence bearing N 20-10-3.917 W a distance of 248.951; thence bearing N 63-9-29.350 E a distance of 909.256; thence bearing N 79-11-36.890 E a distance of 408.664; thence bearing N 89-34-30.900 E a distance of 109.474; thence bearing S 0-38-2.668 E a distance of 83.212; thence bearing S 0-24-45.929 E a distance of 117.500; thence bearing S 0-36-46.181 E a distance of 117.500; thence bearing S 0-24-9.103 E a distance of 124.796; thence bearing N 89-55-7.631 W a distance of 199.655; thence bearing S 0-1-42.588 E a distance of 660.088; thence bearing S 0-36-1.695 E a distance of 71.489; thence bearing S 89-25-42.236 W a distance of 286.048; thence bearing S 89-25-42.236 W a distance of 9.877 to the point of beginning.

Tract 3

Beginning at a point whose Northing is 1367915.845 and whose Easting is 669617.809; thence bearing N 89-25-42.236 E a distance of 286.048; thence bearing N 0-36-1.695 W a distance of 71.489; thence bearing S 89-44-47.620 E a distance of 200.438; thence bearing S 0-1-29.832 E a distance of 184.817; thence along a curve to the LEFT, having a radius of 590.000 a delta angle of 35° 26' 29.50", and whose long chord bears S 35-29-49.749 W a distance of 359.166; thence bearing S 17-46-35.000 W a distance of 99.611; thence bearing N 74-4-59.754 W a distance of 113.102; thence along a curve to the LEFT, having a radius of 475.000 a delta angle of 06° 33' 54.85", and whose long chord bears N 77-21-57.177 W a distance of 54.398; thence bearing N 80-27-18.850 W a distance of 260.447; thence bearing N 9-21-5.406 E a distance of 67.412; thence bearing N 72-42-43.516 E a distance of 54.746; thence bearing N 9-55-42.139 W a distance of 274.571; thence bearing N 71-39-54.955 E a distance of 165.020; thence bearing N 5-14-11.146 W a distance of 7.407 to the point of beginning.

Tract 4

Beginning at a point whose Northing is 1367448.229 and whose Easting is 669755.878; thence bearing S 74-4-59.754 E a distance of 113.102; thence bearing S 17-46-35.000 W a distance of 126.470; thence along a curve to the RIGHT, having a radius of 354.000 a delta angle of 24° 37' 20.10", and whose long chord bears S 30-5-15.051 W a distance of 150.960; thence bearing S 14-15-43.000 W a distance of 34.470; thence along a curve to the RIGHT, having a radius of 371.500 a delta angle of 06° 51' 06.27", and whose long chord bears S 50-31-1.867 W a distance of 44.400; thence bearing S 53-56-35.000 W a distance of 236.593; thence bearing N 55-31-55.505 W a distance of 172.173; thence bearing N 89-25-55.024 W a distance of 216.055; thence bearing S 59-29-30.316 W a distance of 55.004; thence along a curve to the RIGHT, having a radius of 606.263 a delta angle of 42° 28' 51.98", and whose long chord bears N 79-29-5.227 W a distance of 439.279; thence bearing N 27-45-46.000 E a distance of 118.260; thence along a curve to the RIGHT, having a radius of 472.500 a delta angle of 18° 01' 46.95", and whose long chord bears N 36-46-39.477 E a distance of 148.073; thence along a curve to the RIGHT, having a radius of 472.500 a delta angle of 16° 09' 51.75", and whose long chord bears N 53-52-28.830 E a distance of 132.861; thence bearing N 61-57-24.708 E a distance of 115.837; thence along a curve to the RIGHT, having a radius of 472.500 a delta angle of 37° 27' 05.43", and whose long chord bears N 80-40-57.421 E a distance of 303.382 ; thence bearing S 80-35-29.866 E a distance of 115.802; thence bearing S 80-27-18.850 E a distance of 260.447; thence along a curve to the RIGHT, having a radius of 475.000 a delta angle of 06° 33' 54.85", and whose long chord bears S 77-21-57.177 E a distance of 54.398 to the point of beginning.

Tract 5

Beginning at a point whose Northing is 1366745.048 and whose Easting is 668663.609; thence bearing N 74-57-40.102 W a distance of 30.000; thence bearing N 15-2-19.898 E a distance of 50.135; thence bearing N 74-23-16.329 W a distance of 491.651; thence bearing N 37-34-54.164 W a distance of 433.129; thence bearing S 52-27-7.000 W a distance of 120.000; thence bearing N 37-32-53.000 W a distance of 63.420; thence bearing N 52-27-7.000 E a distance of 120.000; thence bearing N 37-32-53.000 W a distance of 262.500; thence bearing N 52-27-43.156 E a distance of 208.463; thence along a curve to the LEFT, having a radius of 1000.769a delta angle of 33° 46' 42.47", and whose long chord bears S 45-21-42.530 E a distance of 581.492; thence bearing S 62-11-29.851 E a distance of 453.554; thence along a curve to the LEFT, having a radius of 666.103a delta angle of 15° 40' 37.68", and whose long chord bears S 70-16-5.261 E a distance of 181.690; thence bearing S 11-12-15.283 W a distance of 46.226; thence bearing S 44-57-8.085 W a distance of 268.948to the point of beginning.

Tract 6

Beginning at a point whose Northing is 1366233.362 and whose Easting is 668008.192; thence bearing N 69-53-8.899 E a distance of 272.187; thence bearing N 49-1-18.527 E a distance of 443.075; thence bearing N 15-2-19.898 E a distance of 140.110; thence bearing S 74-57-40.102 E a distance of 30.000; thence bearing N 44-57-8.085 E a distance of 268.948; thence bearing N 11-12-15.283 E a distance of 46.226; thence along a curve to the LEFT, having a radius of 666.103a delta angle of 11° 18' 04.11", and whose long chord bears S 83-45-26.153 E a distance of 131.171; thence bearing S 0-54-1.000 E a distance of 11.058; thence along a curve to the LEFT, having a radius of 680.200a delta angle of 04° 25' 32.30", and whose long chord bears N 88-23-50.152 E a distance of 52.527; thence bearing N 86-11-4.000 E a distance of 55.403; thence along a curve to the RIGHT, having a radius of 369.670a delta angle of 57° 26' 11.01", and whose long chord bears S 65-5-50.493 E a distance of 355.254; thence bearing S 32-31-36.943 E a distance of 50.012; thence bearing S 60-44-55.000 W a distance of 422.389; thence along a curve to the LEFT, having a radius of 5879.580a delta angle of 08° 44' 58.32", and whose long chord bears S 56-22-25.838 W a distance of 896.988; thence bearing N 89-38-40.621 W a distance of 93.162; thence bearing S 51-14-19.171 W a distance

of 119.719; thence bearing N 89-50-54.000 W a distance of 212.951; thence bearing N 12-52-54.534 W a distance of 61.227; thence bearing N 77-7-5.466 E a distance of 125.000; thence bearing N 12-52-54.534 W a distance of 157.914 to the point of beginning.

Tract 7

Beginning at a point whose Northing is 1366822.680 and whose Easting is 667873.417; thence bearing S 12-52-54.534 E a distance of 604.533; thence bearing S 12-52-54.534 E a distance of 157.914; thence bearing S 77-7-5.466 W a distance of 125.000; thence bearing S 12-52-54.534 E a distance of 61.227; thence bearing N 89-50-54.000 W a distance of 143.519; thence bearing N 0-13-57.000 E a distance of 190.000; thence bearing N 89-46-3.000 W a distance of 110.000; thence bearing N 0-13-57.000 E a distance of 10.000; thence bearing S 89-46-3.000 E a distance of 90.000; thence bearing N 0-13-57.000 E a distance of 60.000; thence bearing N 89-46-3.000 W a distance of 20.000; thence bearing N 0-13-57.000 E a distance of 180.000; thence bearing N 89-46-3.000 W a distance of 44.230; thence bearing N 0-13-57.000 E a distance of 129.980; thence bearing N 89-45-18.311 W a distance of 90.770; thence bearing S 0-13-57.000 W a distance of 50.000; thence bearing N 89-49-55.966 W a distance of 120.007; thence bearing S 0-13-57.000 W a distance of 110.000; thence bearing N 89-46-3.000 W a distance of 85.000; thence bearing S 0-13-57.000 W a distance of 140.000; thence bearing N 74-30-44.000 W a distance of 114.020; thence bearing N 89-46-3.000 W a distance of 190.000; thence bearing S 76-11-46.000 W a distance of 123.690; thence bearing N 89-46-3.000 W a distance of 250.000; thence bearing N 0-13-57.000 E a distance of 62.330; thence along a curve to the RIGHT, having a radius of 138.830 a delta angle of 61° 56' 15.00", and whose long chord bears N 31-12-4.500 E a distance of 142.876; thence bearing N 62-10-11.985 E a distance of 0.003; thence bearing N 62-10-12.000 E a distance of 165.990; thence bearing N 56-26-18.000 E a distance of 360.000; thence bearing N 37-11-42.000 E a distance of 401.480; thence bearing N 51-14-17.000 E a distance of 289.840; thence bearing S 37-18-51.035 E a distance of 169.900; thence bearing S 52-25-1.745 W a distance of 119.087; thence bearing S 37-32-30.858 E a distance of 179.989; thence bearing S 22-7-58.159 E a distance of 59.970 to the point of beginning.

Tract 8

Beginning at a point whose Northing is 1367436.181 and whose Easting is 666551.405; thence bearing S 7-36-17.000 W a distance of 1277.033; thence bearing N 89-46-3.000 W a distance of 165.003; thence bearing N 5-8-48.000 W a distance of 800.000; thence bearing N 11-35-59.000 E a distance of 327.190; thence bearing S 89-46-3.000 E a distance of 12.410; thence bearing N 10-52-60.000 E a distance of 192.440; thence bearing N 45-12-39.000 E a distance of 167.370; thence along a curve to the LEFT, having a radius of 200.000 a delta angle of 40° 24' 38.57", and whose long chord bears S 38-51-27.716 E a distance of 138.154; thence bearing S 59-3-47.000 E a distance of 100.000 to the point of beginning.

Tract 9A

Beginning at a point whose Northing is 1368477.359 and whose Easting is 667502.505; thence along a curve to the RIGHT, having a radius of 796.194 a delta angle of 01° 49' 36.51", and whose long chord bears S 25-53-50.430 E a distance of 25.385; thence along a curve to the RIGHT, having a radius of 35.000 a delta angle of 85° 08' 38.24", and whose long chord bears S 18-13-42.608 W a distance of 47.356; thence bearing S 60-48-1.727 W a distance of 156.126; thence along a curve to the LEFT, having a radius of 350.000 a delta angle of 44° 20' 39.18", and whose long chord bears S 38-37-42.135 W a distance of 264.173; thence along a curve to the RIGHT, having a radius of 309.351 a delta angle of 31° 50' 43.77", and whose long chord bears N 7-56-24.687 W a distance of 169.736; thence bearing N 7-50-5.743 E a distance of 17.977; thence bearing N 12-51-43.890 E a distance of 28.456; thence bearing N 17-53-22.037 E a distance of 28.456; thence bearing N 22-55-0.184 E a distance of 28.456; thence bearing N 27-56-38.331 E a distance of 28.456; thence bearing N 32-58-16.478 E a distance of 28.456; thence bearing N 37-59-54.624 E a distance of 28.456; thence

bearing N 43-1-32.771 E a distance of 28.456; thence bearing N 48-3-10.918 E a distance of 28.456; thence bearing N 53-4-49.065 E a distance of 28.456; thence bearing N 58-6-27.212 E a distance of 28.456; thence bearing N 63-8-5.359 E a distance of 28.456; thence bearing N 65-42-35.946 E a distance of 35.830; thence bearing N 61-30-25.266 E a distance of 5.303; thence bearing N 53-4-16.284 E a distance of 5.303; thence bearing N 44-38-7.301 E a distance of 5.303; thence bearing N 36-11-58.319 E a distance of 5.303; thence bearing N 27-45-49.337 E a distance of 5.433; thence bearing S 43-37-28.028 E a distance of 81.381; thence along a curve to the RIGHT, having a radius of 200.000 a delta angle of 16° 38' 37.32", and whose long chord bears S 35-18-9.369 E a distance of 57.893 to the point of beginning.

Tract 9B

Beginning at a point whose Northing is 1368278.290 and whose Easting is 667497.625; thence bearing S 0-14-21.069 W a distance of 522.623; thence bearing S 63-20-6.000 W a distance of 532.072; thence bearing S 52-41-59.959 W a distance of 253.176; thence bearing N 59-7-48.924 W a distance of 116.481; thence bearing N 30-56-13.000 E a distance of 25.000; thence bearing N 59-3-47.000 W a distance of 70.000; thence bearing N 45-22-54.685 E a distance of 560.063; thence bearing S 28-21-16.794 E a distance of 7.064; thence along a curve to the LEFT, having a radius of 360.000a delta angle of 47° 41' 16.66", and whose long chord bears N 37-48-4.876 E a distance of 291.058; thence along a curve to the RIGHT, having a radius of 290.000a delta angle of 46° 50' 35.18", and whose long chord bears N 37-22-44.136 E a distance of 230.546; thence bearing N 60-48-1.727 E a distance of 145.630; thence along a curve to the RIGHT, having a radius of 35.000a delta angle of 94° 33' 46.24", and whose long chord bears S 71-55-5.153 E a distance of 51.429; thence bearing S 24-38-12.033 E a distance of 64.017; thence bearing N 89-46-0.785 W a distance of 96.839 to the point of beginning.

Tract 10

A legal description for the revised Tract 10 will be provided prior to the City Commission hearing.

Tract 11

A legal description for the revised Tract 11 will be provided prior to the City Commission hearing.

Tract 12 (Formerly part of Area U)

Being a portion of the North ½ of Section 36, Township 27 South, Range 23 East, Polk County, Florida, more particularly described as follows:

Commence at the southwest corner of "Fairfield on the Tee", according of the plat thereof, as recorded in Plat Book 78, Page 24, of the public records of Polk County, Florida, said point being on the arc of a circular curve concave to the northeast and bearing S63°28'10"W from the center of said curve, said point also being on the northerly right-of-way line of Carpenters Way as recorded in official records book 4055, page 644, of the public records of Polk County, Florida; thence southeasterly along the arc of said curve with a radius of 942.44 feet, through a central angle of 13°14'46", a distance of 217.88 feet; thence continue southeasterly, along said northerly right-of-way line and along said curve through a central angle of 22°27'36", a distance of 369.45 feet; thence, continue along said northerly right-of-way line S62°08'12"E (S62°14'14"E per historic deed), a distance of 91.13 feet to the point of beginning; thence N27°25'51"E, a distance of 100.76 feet; thence N64°27'26"E, a distance of 244.38 feet; thence S56°01'07"E (S56°00'03"E per historic deed), a distance of 170.71 feet; thence S32°43'25"E (S32°50'30" per historic deed), a distance of 48.04 feet (48.13' per historic deed); thence S55°22'26"E (S55°24'36"E per historic deed), a distance of 161.05 feet (161.03' per historic deed); thence S75°20'55"W (S75°20'02"W per historic deed), a distance of 154.15 feet (154.24' per historic deed); thence S31°57'55"W (S31°57'46"W per historic deed), a distance of 45.20 feet (45.17' per historic deed); thence S13°34'12"W (S13°26'54"W per historic deed), a distance of 57.17 feet (57.26'

per historic deed); thence $S08^{\circ}49'51"E$ ($S09^{\circ}39'34"E$ per historic deed), a distance of 33.77 feet (33.20' per historic deed) to the point of curvature of a non tangent curve to the right, having a radius of 599.21 feet, a central angle of $06^{\circ}51'38"$ ($06^{\circ}52'14"$ per historic deed), a chord bearing of $N65^{\circ}42'41"W$, for a chord distance of 71.71 feet; thence along the arc of said curve, a distance of 71.75 feet (71.85' per historic deed); thence $N62^{\circ}08'12"W$ ($N62^{\circ}14'14"W$ per historic deed), a distance of 362.30 feet to the point of beginning, and containing 2.841 acres (108.073 square feet) of land, more or less.

Exhibit "W-2"

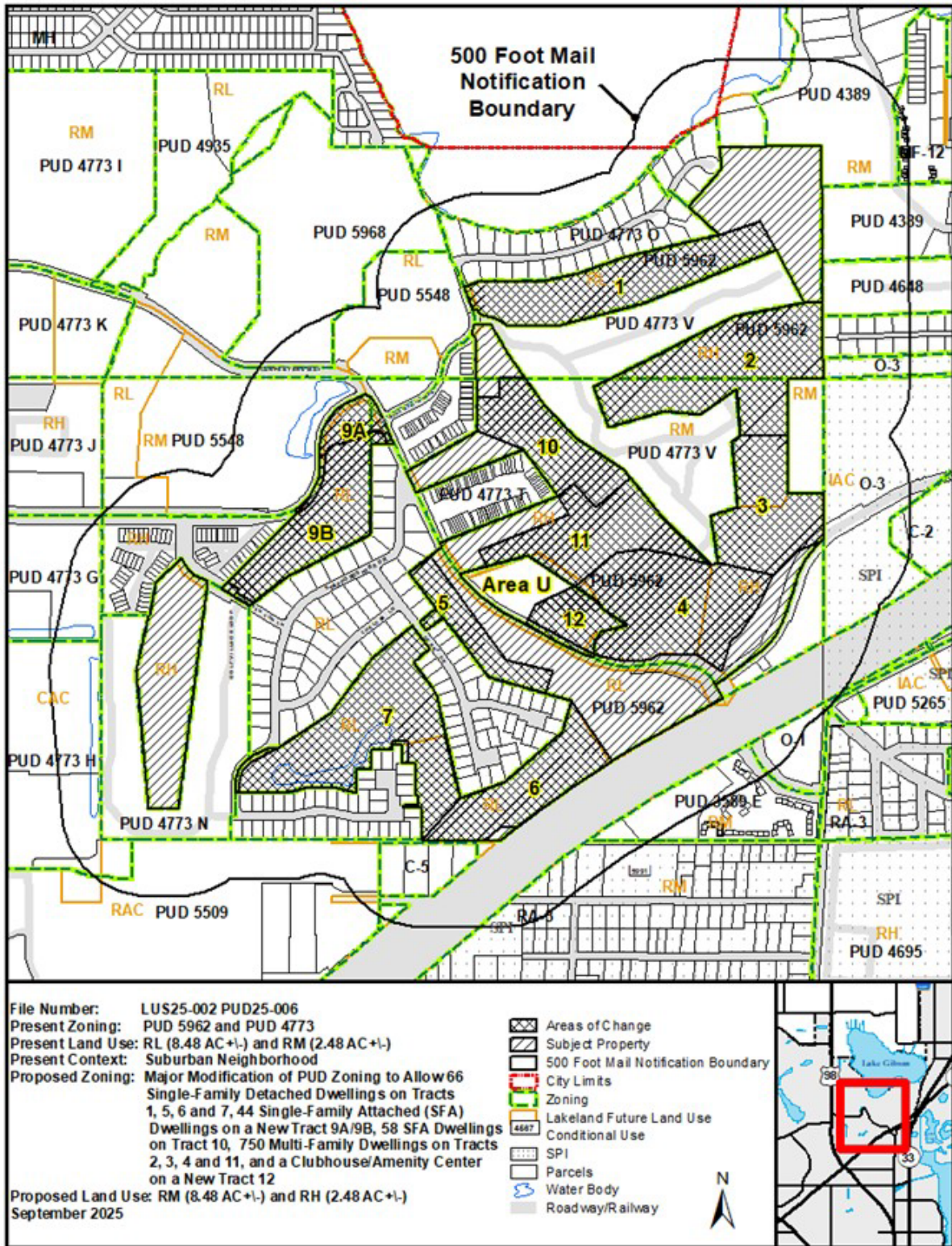


Exhibit "W-3"

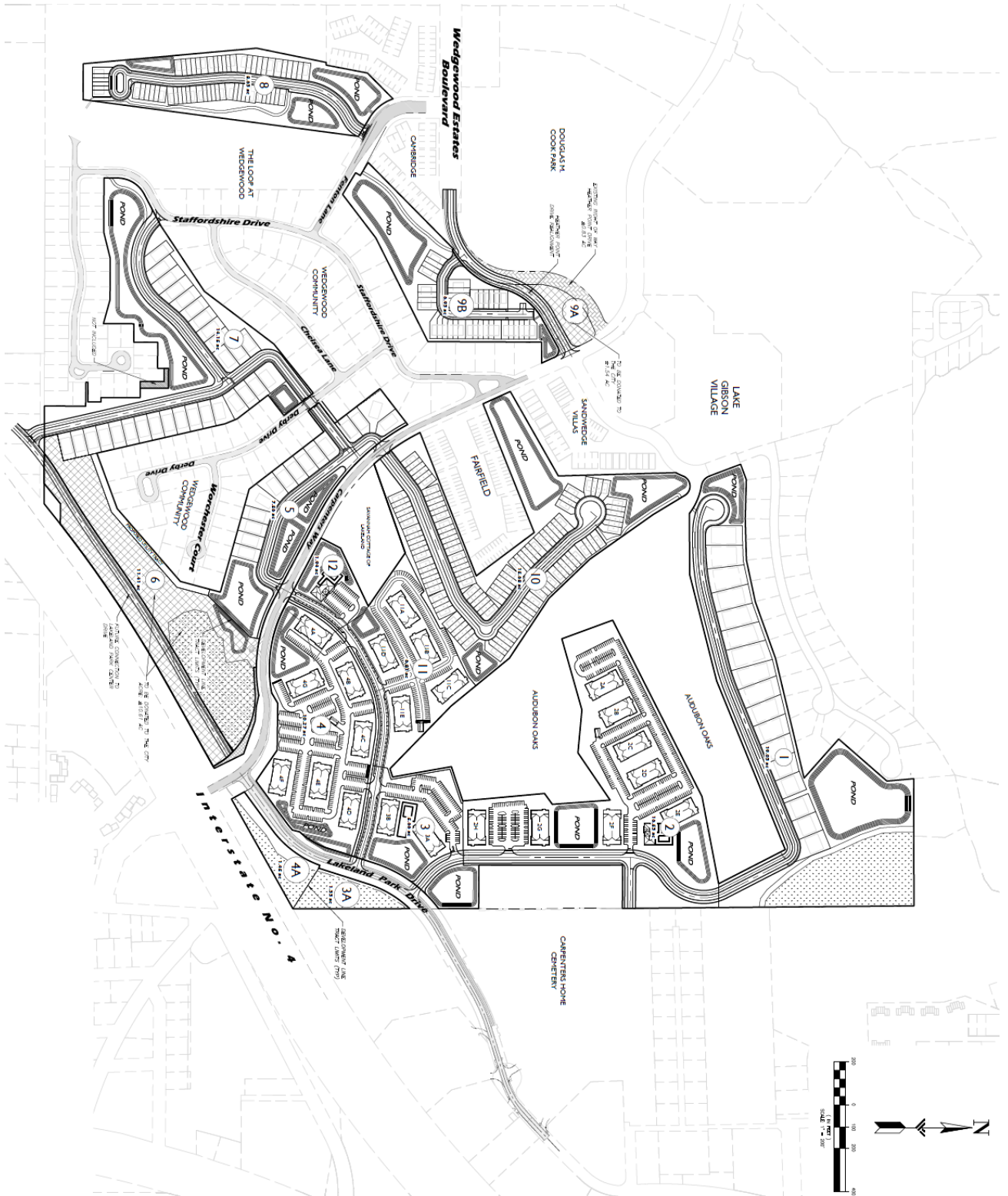
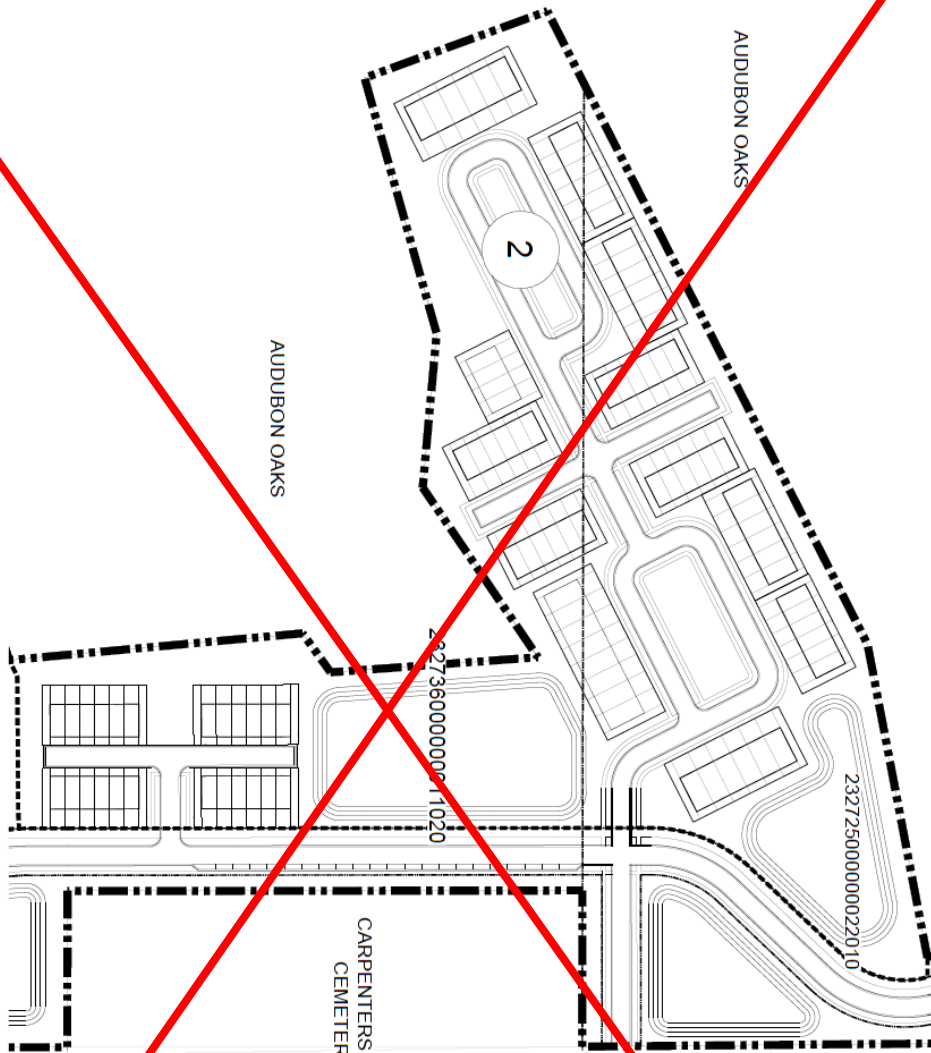


Exhibit "W-3a"



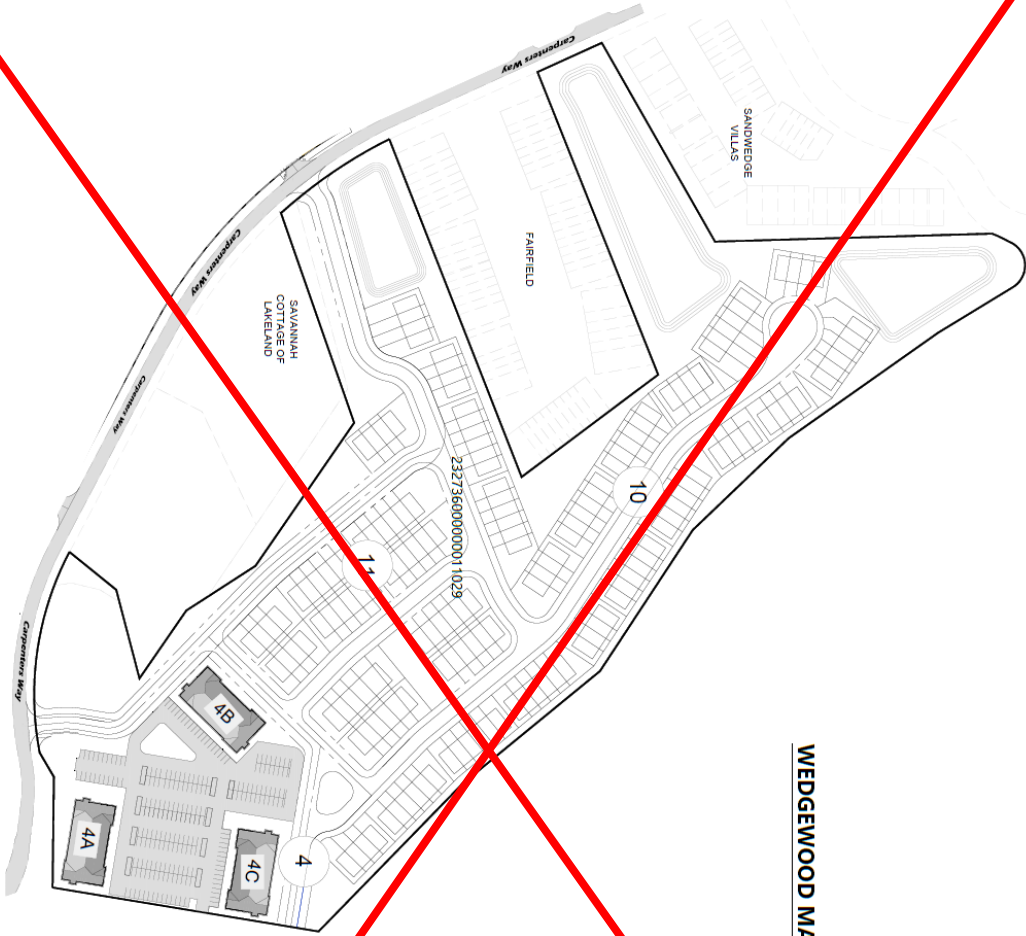
TOWNHOME PARCELS

DEVELOPMENT AREA 2	EXISTING F.U.	PROPOSED F.U.	PHASE
DEVELOPMENT AREA 2	RM	RM1	III
TOTAL ACRES			
TOTAL UNITS	2	36 UNITS	
DENSITY (22' X 75')		18 UNITS	
DENSITY (19' X 77')		36 UNITS	
DENSITY (5' X 50')		36 UNITS	

WEDGWOOD MASTER PLAN - DEVELOPMENT AREA 2

NOVEMBER 8, 2023





WEDGEWOOD MASTER PLAN - DEVELOPMENT AREA 4, 10 & 11

PRELIMINARY SITE DATA TABLE

MULTI-FAMILY PARCELS

DEVELOPMENT AREA 4:
 UNITS ± 120
 PARKING REQUIRED ± 216 (1.8 RATIO)
 1 BEDROOM / STUDIO ± 81 (1.5 PER UNIT)
 2+ BEDROOM ± 72 (2 PER UNIT)

BUILDING DATA	PHASE
4B 4-STORY / 40 UNITS	II
4C 4-STORY / 40 UNITS	II

TOWNHOMES PARCELS

DEVELOPMENT AREA 10 & 11:
 UNITS ± 180 UNITS
 (22 X 87)
 (19 X 77)
 TOTAL UNITS ± 180 UNITS

EXISTING BLU	PHASE
NM	NM

DEVELOPMENT AREA 4, 10 & 11:
 TOTAL UNITS ± 300 UNITS
 DENSITY ± 9.0 DU/AC

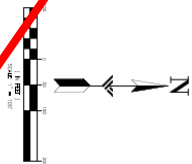
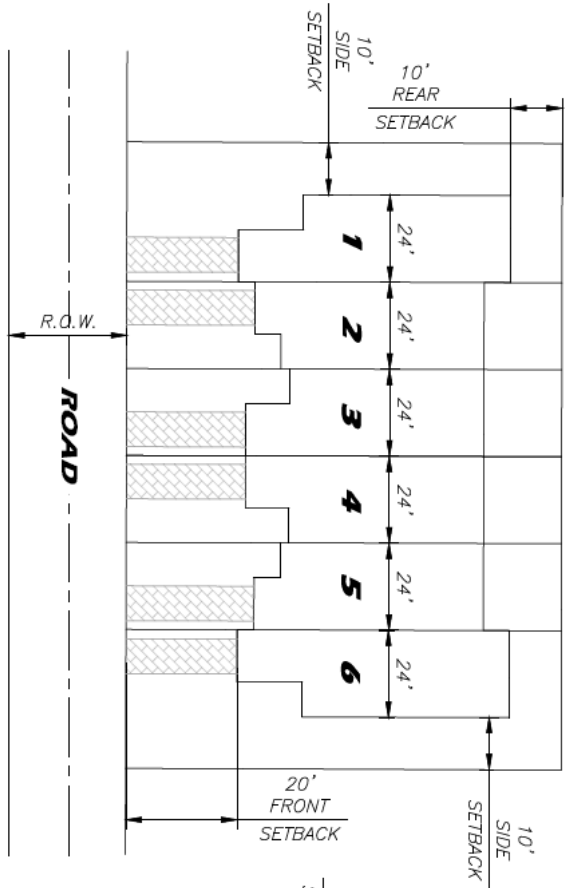


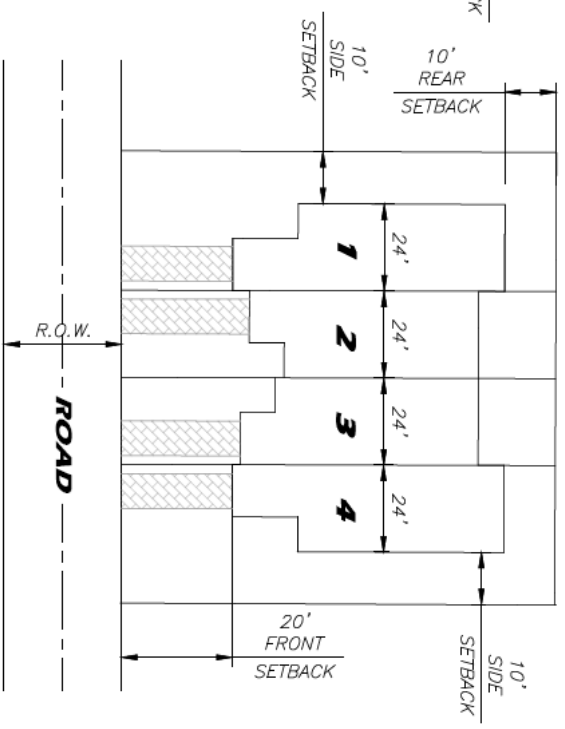
Exhibit "W-3a"

LOT LAYOUT TOWNHOUSE



**LOT LAYOUT
6 UNITS TOWNHOUSE**
N.T.S.

NOTE: GARAGES SHALL BE RECESSED TO 25 FEET



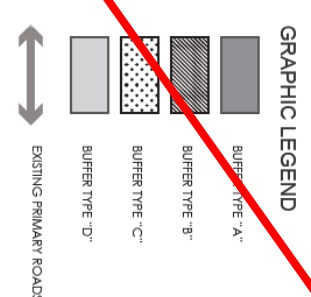
**LOT LAYOUT
4 UNITS TOWNHOUSE**
N.T.S.

NOTE: GARAGES SHALL BE RECESSED TO 25 FEET

Exhibit "W 4a"

LANDSCAPE BUFFERS (EXHIBIT W-4A)

DECEMBER 7, 2022



SCALE IN FEET
0 100 200 400

Kimley-Horn

Exhibit "W-4a"

3

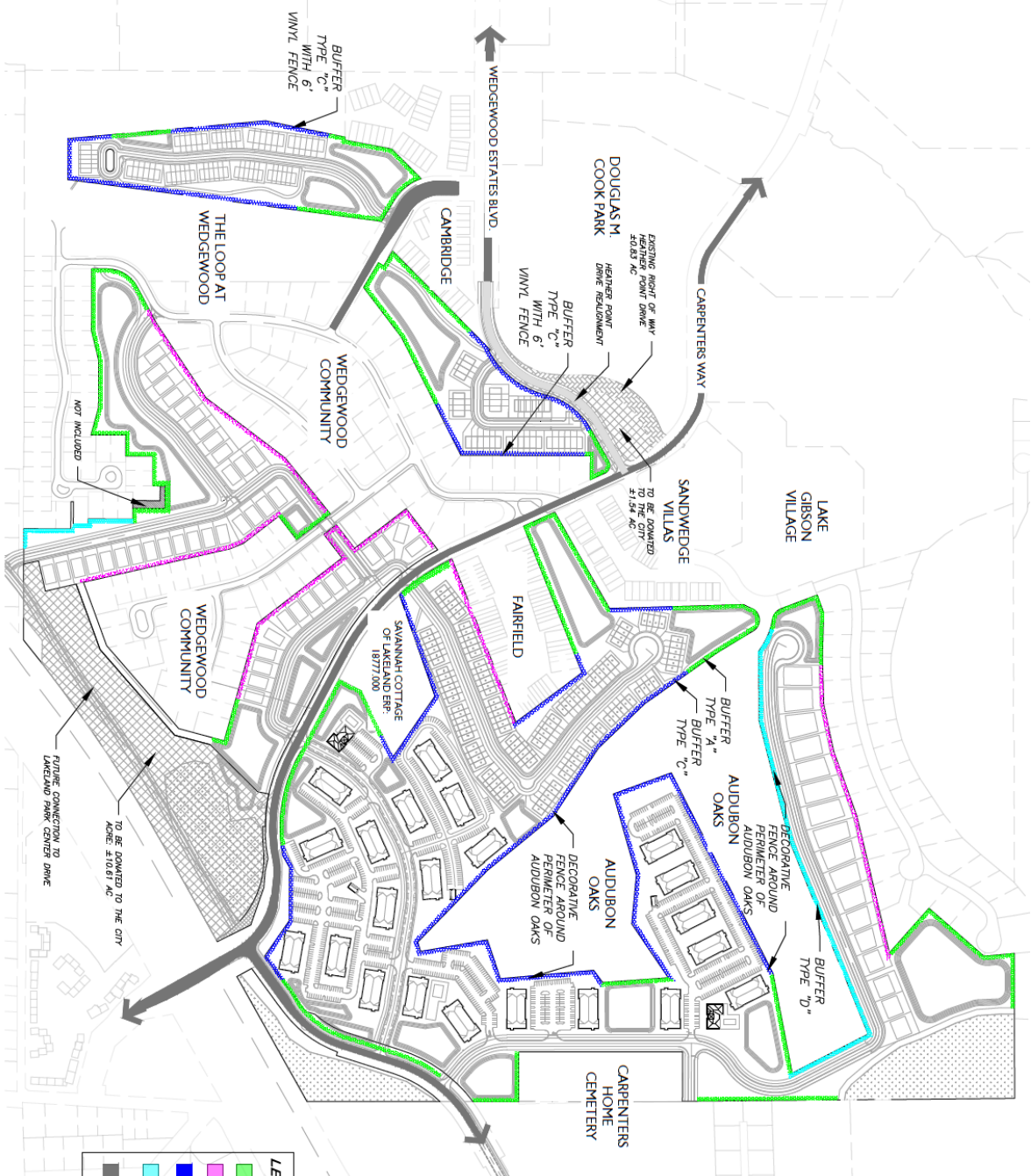
LANDSCAPE BUFFERS

ATTACHMENT "W-4a"

SCALE: 1" = 350'

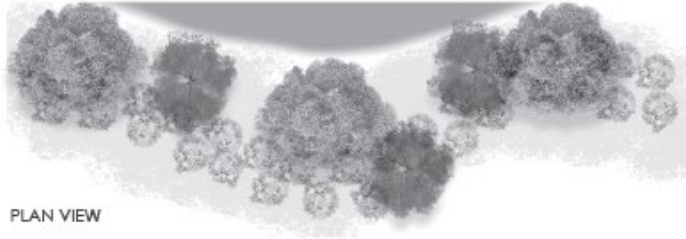


TRACT ENGINEERING
FROM CONCEPTUAL TO COMPLETION
3010 SANDHURST DRIVE
SAN ANTONIO, TEXAS 78245
PHONE: (214) 480-1444



LEGEND	
█	BUFFER TYPE "A"
█	BUFFER TYPE "B"
█	BUFFER TYPE "C"
█	BUFFER TYPE "D"
█	EXISTING PRIMARY ROADS

Exhibit "W-4b"



A TYPE 'A' LANDSCAPE BUFFER

TYPICAL PER EVERY 100 LF

- 3 canopy trees, 3 understory trees, 30 shrubs, 2'-3' height berm
- shrubs to be placed in natural, organic patterns

PLAN VIEW



B TYPE 'B' LANDSCAPE BUFFER

TYPICAL PER EVERY 100 LF

- 4 canopy trees, 6 understory trees, 50 shrubs, 2'-3' height berm
- shrubs to be placed in natural / organic patterns

PLAN VIEW

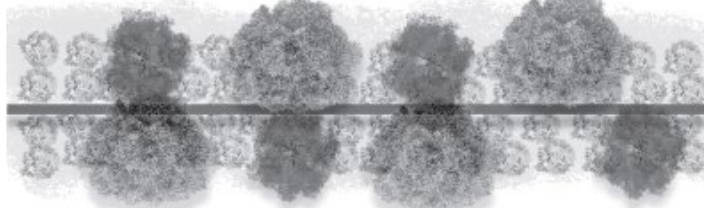


C TYPE 'C' LANDSCAPE BUFFER

TYPICAL PER EVERY 100 LF

- 2 canopy trees, 2 understory trees, 30 shrubs
- shrubs to be placed in natural / organic patterns

PLAN VIEW



D TYPE 'D' LANDSCAPE BUFFER

TYPICAL PER EVERY 100 LF

- 4 trees, 4 understory trees, 30 shrubs (each side of wall), optional 6' ht. screen wall

LANDSCAPE BUFFER - SECTIONS



GRAPHIC LEGEND

- SECTION "A"
- SECTION "B"
- SECTION "C"
- EXISTING PRIMARY ROADS
- EXISTING SECONDARY ROADS

SITE CIRCULATION - ROADWAY (EXHIBIT W-5A)

DECEMBER 6, 2022

SCALE: 1" = 500'

Kimley-Horn

DECEMBER 6, 2022
SITE CIRCULATION - PEDESTRIAN (EXHIBIT W-5B)



GRAPHIC LEGEND

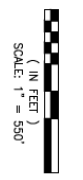
- SECTION 'D'
- ↔ EXISTING PRIMARY ROADS



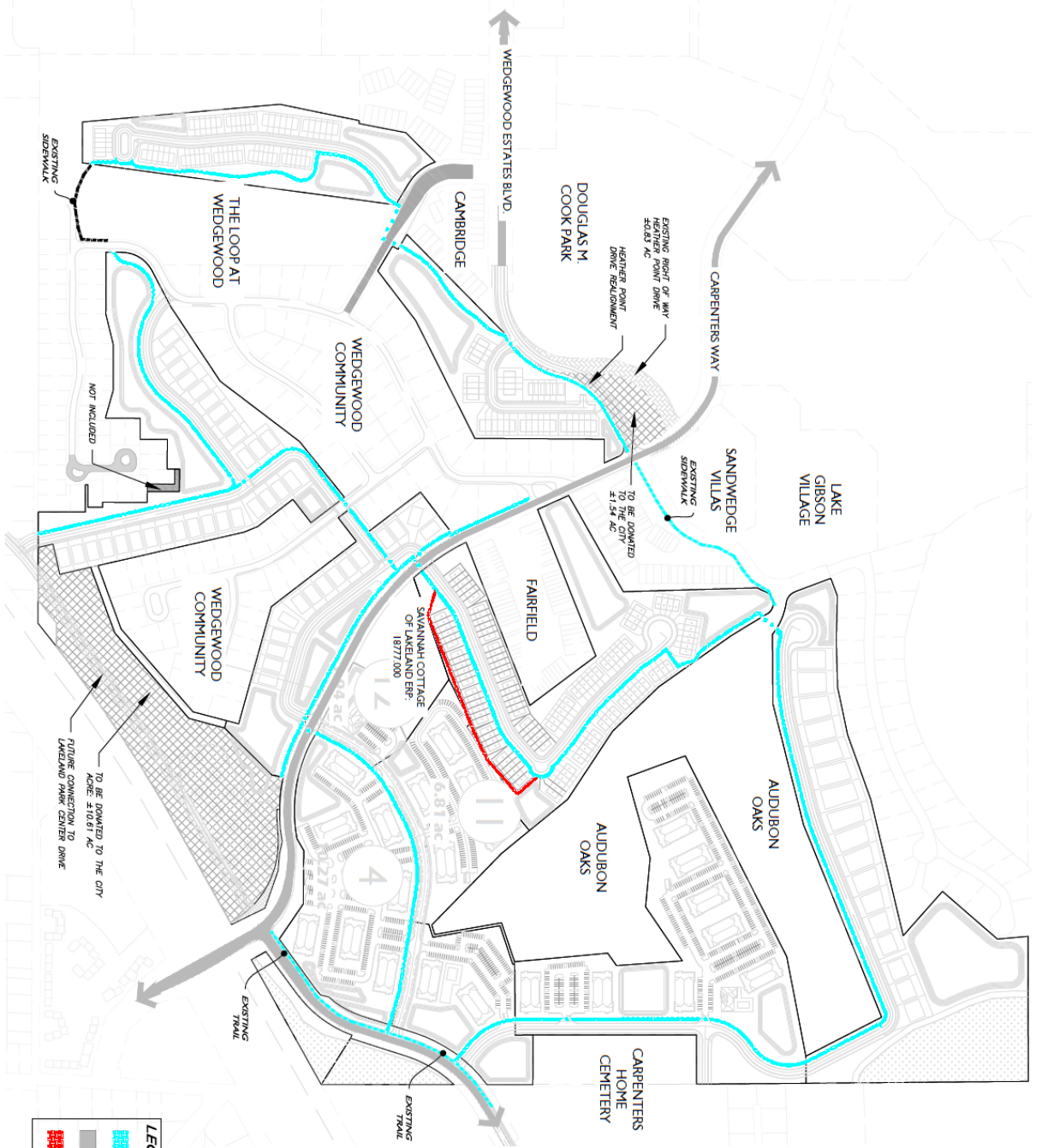
Kimley»Horn

SITE CIRCULATION - PEDESTRIAN

ATTACHMENT "W-5b"



TRACT ENGINEERING
FROM CONCEPTUAL TO COMPLETION
5117 LAKELAND BL, SUITE 100
ANN ARBOR, MI 48106-1440
PHONE: 734.769.8800



LEGEND

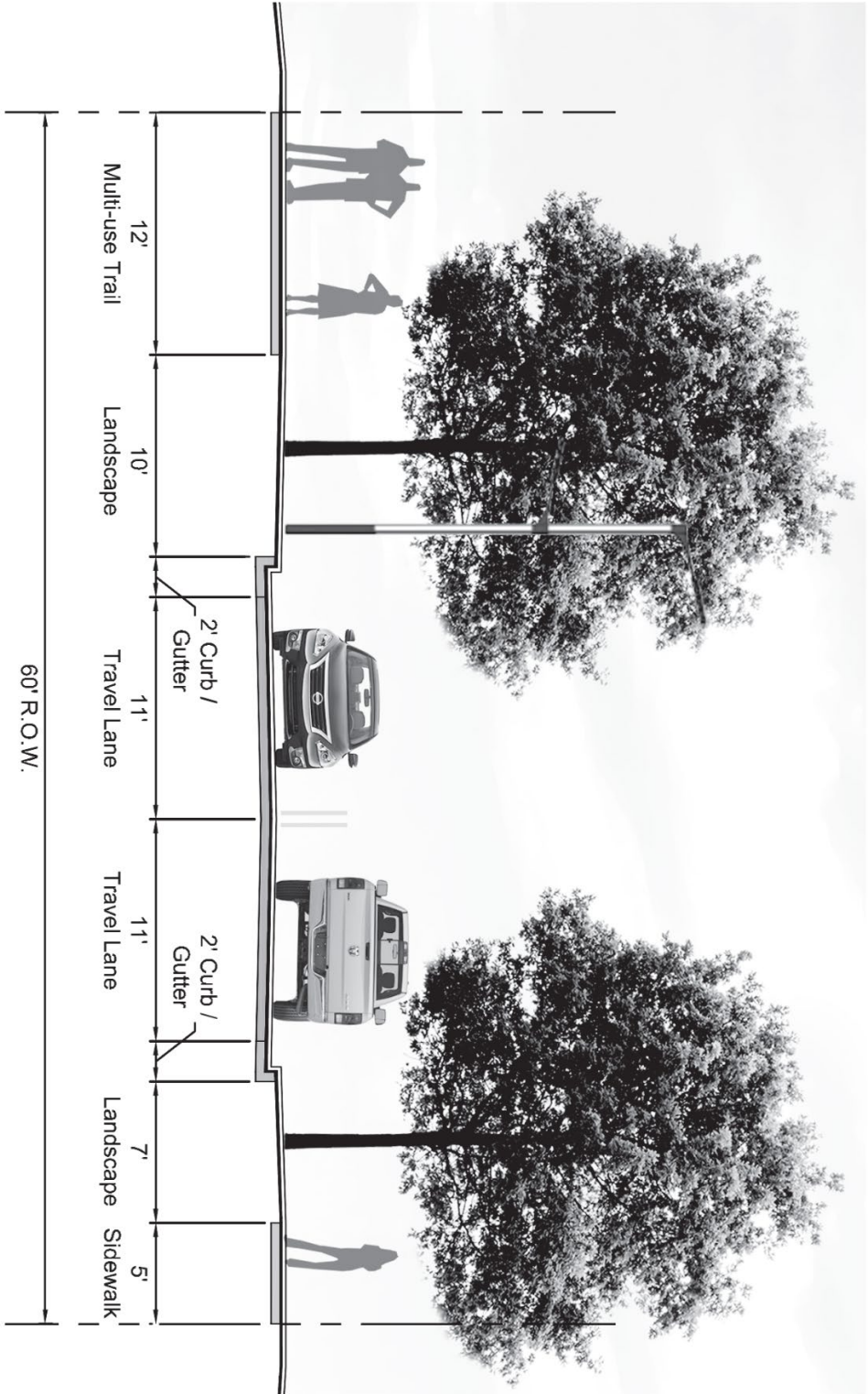
	SECTION "B"
	EXISTING PRIMARY ROADS
	SECTION "D1"

Exhibit "W-6a"



A

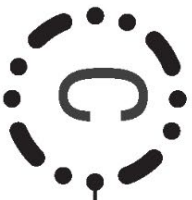
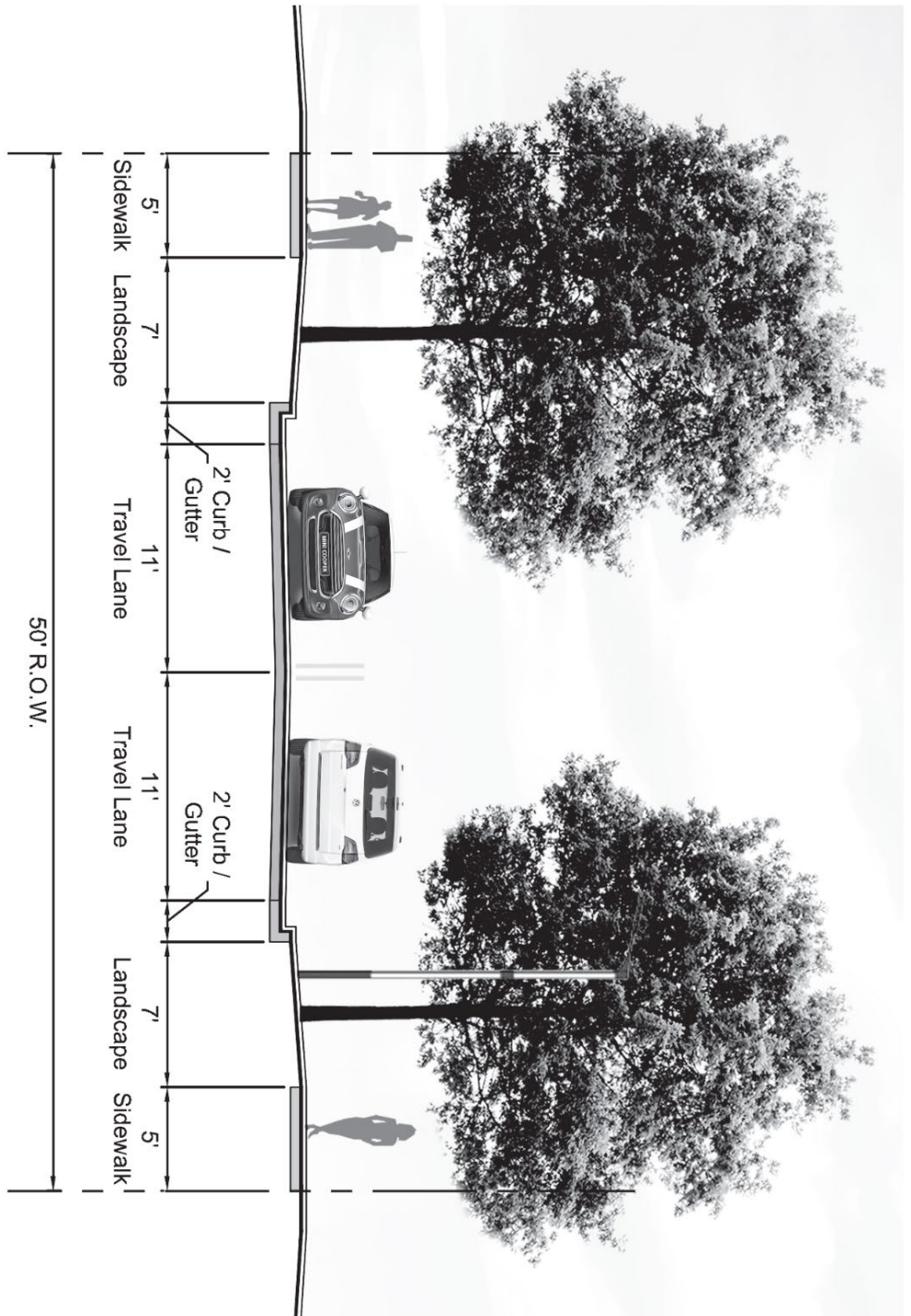
SECTION 'A'
(64' R.O.W. with 2-way traffic, on-street parking, multi-use trail, and landscaping)



SECTION 'B'

(60' R.O.W. with 2-way traffic, multi-use trail, and landscaping)

Exhibit "W-6c"



SECTION 'C'

(50' R.O.W. with 2-way traffic, standard sidewalk, and landscaping)

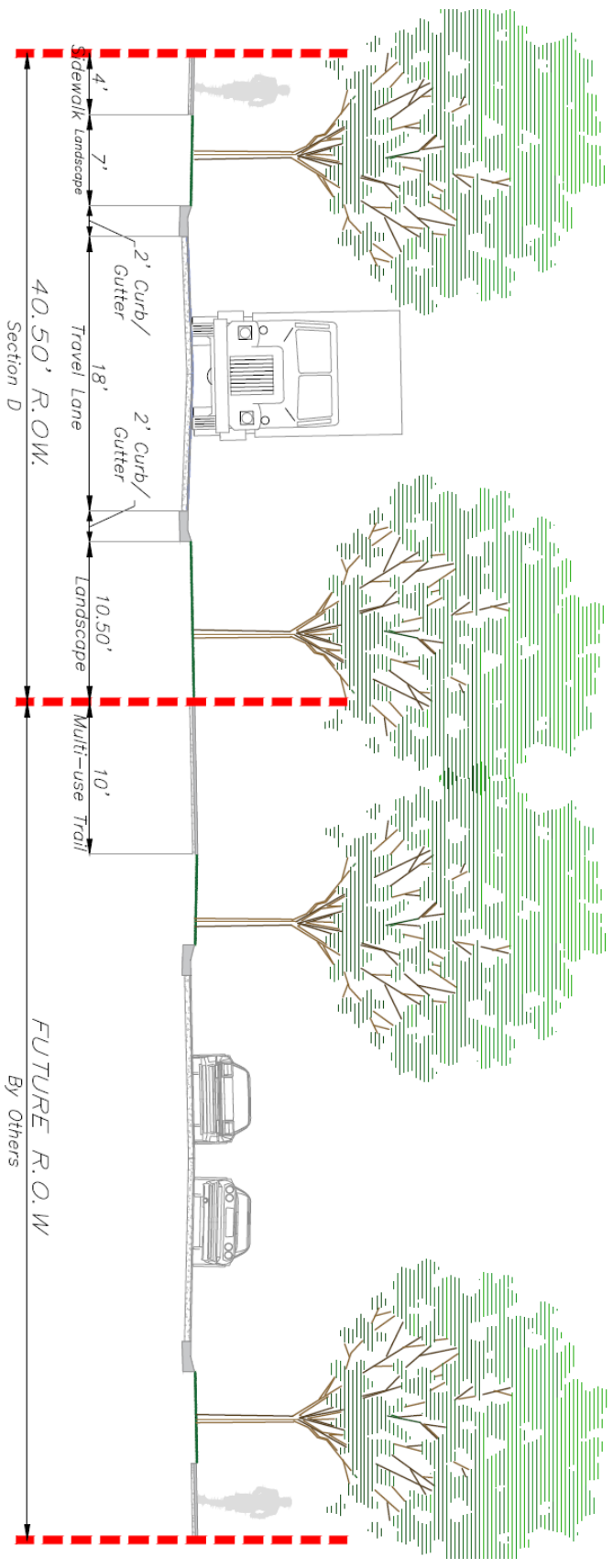


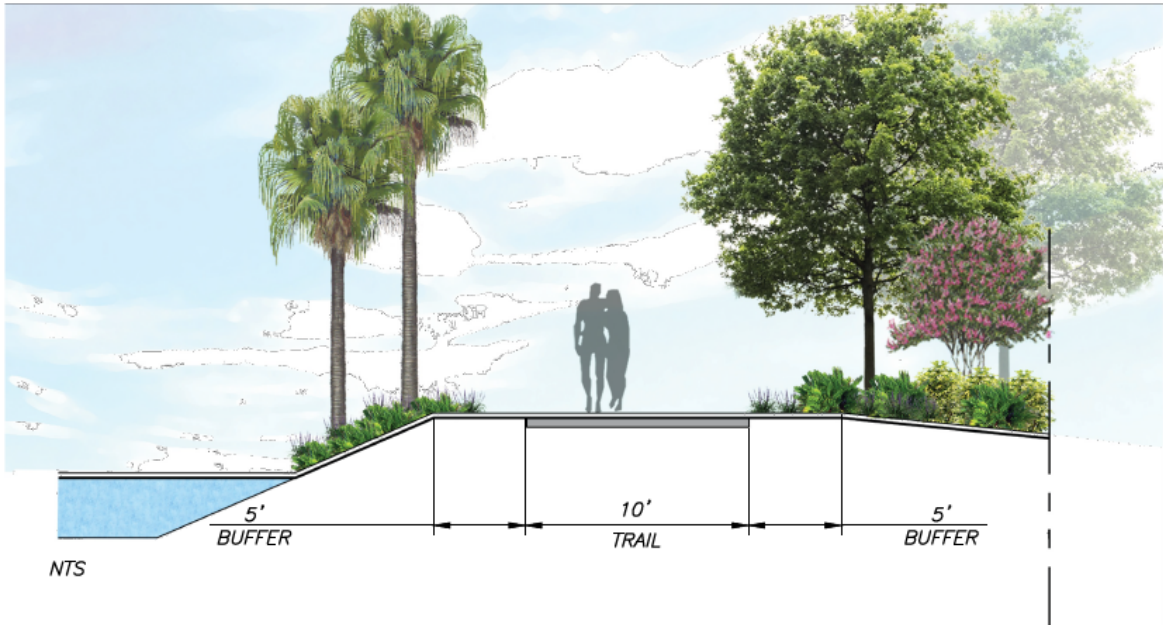
SECTION 'D'

(multi-use trail / greenway open space)

SECTION "E" — ROW

Exhibit "W-6e"





SECTION "D1"

**WEDGE
WOOD**

TRACT ENGINEERING
FROM CONCEPTUAL TO COMPLETION



5137 S LAKELAND DR, SUITE 3
LAKELAND, FL 33813
FIRM REGISTRATION NUMBER - 34343

Exhibit "W-7"

PRELIMINARY SITE DATA TABLE

SINGLE FAMILY PARCELS

		EXISTING FLU	PROPOSED FLU	PHASE
DEVELOPMENT AREA 1:				
TOTAL AREA	± 19.04 AC	RM	RL	II
WETLANDS	± 3.33 AC			
DEVELOPABLE AREA:	± 15.71 AC			
UNITS (90' WIDE):	± 14			
DENSITY:	± 0.89 DU/AC			
DEVELOPMENT AREA 5:				
TOTAL AREA	± 13.02 AC	RL	RL	I
WETLANDS	± 2.99 AC			
ROW DEDICATION	± 1.31 AC			
DEVELOPABLE AREA:	± 8.72 AC			
UNITS (60' WIDE):	± 14			
DENSITY:	± 1.61 DU/AC			
DEVELOPMENT AREA 6:				
TOTAL AREA	± 9.18 AC	RL	RL	I
ROW DEDICATION	± 1.42 AC			
DEVELOPABLE AREA:	± 7.76 AC			
UNITS (60' WIDE):	± 12			
DENSITY:	± 1.55 DU/AC			
DEVELOPMENT AREA 7:				
DEVELOPABLE AREA:	± 11.21 AC	RL	RL	I
UNITS (60' WIDE):	± 20			
DENSITY:	± 1.78 DU/AC			
TOTAL SINGLE FAMILY UNITS:	± 60 UNITS			

TOWNHOME PARCELS

		EXISTING FLU	PROPOSED FLU	PHASE
DEVELOPMENT AREA 8:				
DEVELOPABLE AREA	± 8.63 AC	RH	RH	II
TOTAL UNITS:	± 60 UNITS			
(24' X 83')	± 12 UNITS			
(22' X 76')	± 19 UNITS			
(19' X 77')	± 36 UNITS			
DENSITY:	± 6.95 DU/AC			
TOTAL TOWNHOME UNITS:	± 60 UNITS			

MULTIFAMILY PARCELS

DEVELOPMENT AREA 2:				
DEVELOPABLE AREA:	± 16.55 AC	BUILDING DATA:		PHASE
UNITS:	± 230 ←	2A	4-STORY / 40 UNITS	III
DENSITY:	± 13.90 DU/AC	2B	4-STORY / 40 UNITS	III
PARKING PROVIDED:	± 295 (2.2 RATIO)	2C	3-STORY / 30 UNITS	III
PARKING REQUIRED:	± 414	2D	3-STORY / 30 UNITS	III
1 BEDROOM/STUDIO	± 92 (1.5 PER UNIT)	2E	3-STORY / 30 UNITS	III
2+ BEDROOM	± 138 (2 PER UNIT)	2F	3-STORY / 30 UNITS	III
		2G	3-STORY / 30 UNITS	III
DEVELOPMENT AREA 3:				
TOTAL AREA	± 12.46 AC	BUILDING DATA:		PHASE
WETLANDS	± 2.77 AC	3A	4-STORY / 40 UNITS	II
DEVELOPABLE AREA:	± 9.69 AC	3B	4-STORY / 40 UNITS	II
UNITS:	± 154 ←	3C	4-STORY / 34 UNITS	II
DENSITY:	± 15.89 DU/AC	3D	4-STORY / 40 UNITS	II
PARKING PROVIDED:	± 264 (1.7 RATIO)			
PARKING REQUIRED:	± 277			
1 BEDROOM/STUDIO	± 62 (1.5 PER UNIT)			
2+ BEDROOM	± 92 (2 PER UNIT)			
DEVELOPMENT AREA 4/10/11:				
DEVELOPABLE AREA:	± 28.31 AC	BUILDING DATA:		PHASE
UNITS:	± 450 ←	4A	4-STORY / 40 UNITS	II
DENSITY:	± 15.90 DU/AC	4B	4-STORY / 40 UNITS	II
PARKING PROVIDED:	± 861 (1.9 RATIO)	4C	4-STORY / 40 UNITS	II
PARKING REQUIRED:	± 810	4D	4-STORY / 40 UNITS	I
1 BEDROOM/STUDIO	± 180 (1.5 PER UNIT)	4E	4-STORY / 40 UNITS	I
2+ BEDROOM	± 270 (2 PER UNIT)	4F	4-STORY / 40 UNITS	I
		4G	4-STORY / 40 UNITS	I
		10A	2-STORY / 20 UNITS	I
		10B	2-STORY / 20 UNITS	I
		10C	2-STORY / 20 UNITS	I
		11A	4-STORY / 40 UNITS	I
		11B	4-STORY / 40 UNITS	I
		11C	3-STORY / 30 UNITS	I
TOTAL MULTIFAMILY UNITS:	± 834 UNITS			
BUILDINGS (80' X 157')	± 24			
PHASE 1	± 330 UNITS			
PHASE 2	± 274 UNITS			
PHASE 3	± 230 UNITS			

Exhibit "W-7"

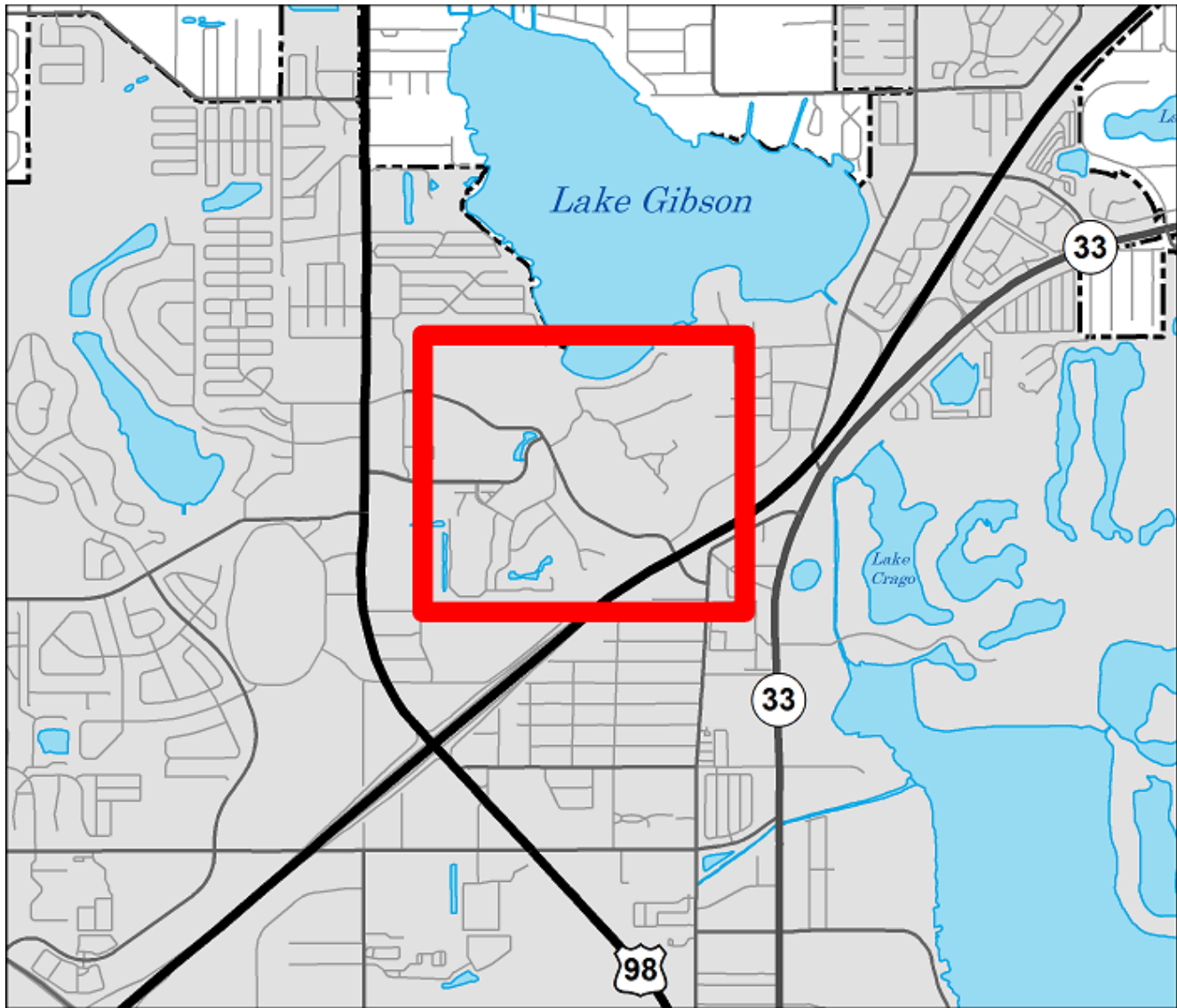
PRELIMINARY SITE DATA TABLE					MULTI-FAMILY				
					EXISTING FLU	PROPOSED FLA	PHASE		
DETACHED SINGLE FAMILY (MAX. 2 STORY)					EXISTING FLU	PROPOSED FLA	PHASE		
DEVELOPMENT AREA 1					RL	RL	II		
TOTAL AREA	19.03 AC								
WETLANDS	3.79 AC								
DEVELOPABLE AREA	15.24 AC								
UNITS (60' WIDE)	17 DU								
DENSITY	0.89 DU/AC								
DEVELOPMENT AREA 5					RL	RL	II		
TOTAL AREA	7.56								
WETLANDS	0								
DEVELOPABLE AREA	7.56								
UNITS (60' WIDE)	15								
DENSITY	1.98								
DEVELOPMENT AREA 6					RL	RL	II		
TOTAL AREA	11.41 AC								
WETLANDS	3.25 AC								
DEVELOPABLE AREA	8.16 AC								
UNITS (60' WIDE)	0 DU								
DENSITY	0.00 DU/AC								
DEVELOPMENT AREA 7					RL	RL	II		
TOTAL AREA	14.15 AC								
WETLANDS	0 AC								
DEVELOPABLE AREA	14.15 AC								
UNITS (60' WIDE)	29 DU								
DENSITY	2.05 DU/AC								
TOTAL SF UNITS	61								
ATTACHED SINGLE FAMILY (MAX. 2 STORY)					EXISTING FLU	PROPOSED FLA	PHASE		
DEVELOPMENT AREA 8					RM	RM	II		
TOTAL AREA	8.63 AC								
WETLANDS	0 AC								
DEVELOPABLE AREA	8.63 AC								
UNITS (TH)	60 DU								
DENSITY	6.95 DU/AC								
DEVELOPMENT AREA 9					RL	RM	II		
TOTAL AREA	8.48 AC								
WETLANDS	0 AC								
DEVELOPABLE AREA	8.48 AC								
UNITS (TH)	36 DU								
DENSITY	4.25 DU/AC								
DEVELOPMENT AREA 10					RM	RM	II		
TOTAL AREA	15.68 AC								
OPEN SPACE	6.40 AC								
WETLANDS	0 AC								
DEVELOPABLE AREA	15.68 AC								
UNITS (TH)	116 DU								
DENSITY	7.40 DU/AC								
TOTAL TH UNITS	212								
DEVELOPMENT AREA 2 (MAX 3-STORY)					RM	RH	II		
TOTAL AREA	16.53 AC								
WETLANDS	0 AC								
DEVELOPABLE AREA	16.53 AC								
UNITS	240 DU (30/BLDG/3-STORY)								
DENSITY	14.52 DU/AC								
PARKING REQUIRED (1 BLDG = 12/15/3)									
1 BED (1.5 SPA / BED)	144								
2+ BED (2 SPA / BED)	240								
3+ BED (2 SPA / BED)	48								
TOTAL SPACES	432								
PARKING PROVIDED	502								
DEVELOPMENT AREA 3 (MAX 3-STORY)					RH	RH	I		
TOTAL AREA	6.99 AC								
OPEN SPACE	1.27 AC								
WETLANDS	1.2 AC								
DEVELOPABLE AREA	5.79 AC								
UNITS	60 DU (30/BLDG/3-STORY)								
DENSITY	8.58 DU/AC								
PARKING REQUIRED (1 BLDG = 12/15/3)									
1 BED (1.5 SPA / BED)	36								
2+ BED (2 SPA / BED)	60								
3+ BED (2 SPA / BED)	12								
TOTAL SPACES	108								
PARKING PROVIDED	161								
DEVELOPMENT AREA 4 (MAX 4-STORY)					RH	RH	I		
TOTAL AREA	11.72 AC								
OPEN SPACE	3.60 AC								
WETLANDS	0 AC								
DEVELOPABLE AREA	11.72 AC								
UNITS	280 DU (40/BLDG)								
DENSITY	23.89 DU/AC								
PARKING REQUIRED (1 BLDG = 12/15/3)									
1 BED (1.5 SPA / BED)	168								
2+ BED (2 SPA / BED)	280								
3+ BED (2 SPA / BED)	56								
TOTAL SPACES	504								
PARKING PROVIDED	504								
DEVELOPMENT AREA 11 (MAX 3-STORY)					RM	RH	I		
TOTAL AREA	6.81 AC								
OPEN SPACE	2.18 AC								
WETLANDS	0 AC								
DEVELOPABLE AREA	6.81 AC								
UNITS (60' WIDE)	120 DU (30/BLDG)								
DENSITY	17.62 DU/AC								
PARKING REQUIRED (1 BLDG = 12/15/3)									
1 BED (1.5 SPA / BED)	72								
2+ BED (2 SPA / BED)	120								
3+ BED (2 SPA / BED)	24								
TOTAL SPACES	216								
PARKING PROVIDED	270								
DEVELOPMENT AREA 12 (MAX 3-STORY)					RM	RH	I		
TOTAL AREA	1.94 AC								
OPEN SPACE	1.00 AC								
WETLANDS	0 AC								
DEVELOPABLE AREA	1.94 AC								
USE = CLUBHOUSE									
DENSITY	0.00 DU/AC								
PARKING REQUIRED (1 BLDG = 12/15/3)									
1 BED (1.5 SPA / BED)	0								
2+ BED (2 SPA / BED)	0								
3+ BED (2 SPA / BED)	0								
TOTAL SPACES	0								
PARKING PROVIDED	20								
TOTAL MF UNITS	700								
TOTAL PARKING REQUIRED	620								
TOTAL PARKING PROVIDED	1383								
TOTAL SITE SUMMARY									
TOTAL AREA	128.93 AC								
WETLANDS	8.24 AC								
DEVELOPABLE AREA	120.69 AC								
TOTAL UNITS	973 DU								
DENSITY	7.55 DU/AC								
TOTAL STORMWATER	26 AC	20%							



Planning & Zoning Board Recommendation

Date:	December 1, 2025	Reviewer:	Todd Vargo
Project No:	LUS25-002 PUD25-006	Location:	Former Wedgewood Golf Course
Owner:	SJD Development LLC		
Applicant:	Chloe Dyal and Chirag Kikani, Tract Engineering		
Current Zoning:	PUD (Planned Unit Development)	Future Land Use:	Residential Low (RL), Residential Medium (RM), Residential High (RH)
Context District:	Suburban Neighborhood (SNH)		
P&Z Hearing:	September 16, 2025	P&Z Final Decision:	November 18, 2025
Request:	<p>Small-scale map amendment and major modification of PUD (Planned Unit Development) zoning on property generally located north of Interstate-4, south of Heatherpoint Drive, east and west of Carpenters Way, and south of Wedgewood Estates Boulevard. More specifically, the requested changes are as follows:</p> <ol style="list-style-type: none"> I. A small-scale map amendment, to change the future land use designation from Residential Low (RL) to Residential Medium (RM), and a major modification of PUD zoning to allow 36 single-family attached (townhome) dwelling units on approximately 8.48 acres located at 752 Carpenters Way (Tract 9A and 9B within Area W). II. A small-scale map amendment, to change the future land use designation from Residential Medium (RM) to Residential High (RH), and a major modification of PUD zoning to remove a portion of Area U and create a new Tract 12 in Area W for a clubhouse/amenity center to serve future multi-family residential uses on Tracts 4 and 11, in lieu of a previously approved 70,100 sq. ft. assisted living facility, on approximately 2.48 acres located at 503 Carpenters Way (Area U). III. A major modification of PUD zoning to increase the maximum number of single-family detached dwelling units from 14 to 17 on approximately 19.02 acres located south of Heatherpoint Drive, east of Carpenters Way, and north of Audubon Oaks Drive (Tract 1 within Area W). IV. A major modification of PUD zoning to adopt a new site development plan and reduce the maximum number of single-family detached dwelling units from 46 to 44 on approximately 33.24 acres generally located north of Interstate-4, south of Wedgewood Estates Boulevard, and west of Carpenters Way (Tracts 5, 6, and 7 within Area W). V. A major modification of PUD zoning to adopt a new site plan to allow 116 single-family attached (townhome) dwelling units and 700 multi-family dwelling units, in lieu of previously approved entitlements for 834 multi-family dwelling units, on approximately 57.74 acres located north of Lakeland Park Center Drive, east of Carpenters Way, east and west of Audubon Oaks apartments (Tracts 2, 3, 4, 10 and 11 within Area W). 		

1.0 Location Maps



2.0 Background

2.1 Summary

Chloe Dyal and Chirag Kikani, on behalf of SJD Development LLC, requests a small-scale map amendment and major modification of PUD (Planned Unit Development) zoning on property generally located north of Interstate-4, south of Heatherpoint Drive, east and west of Carpenters Way, and south of Wedgewood Estates Boulevard. More specifically, the requested changes are as follows:

- I. A small-scale map amendment, to change the future land use designation from Residential Low (RL) to Residential Medium (RM), and a major modification of PUD zoning to allow 36 single-family attached (townhome) dwelling units on approximately 8.48 acres located at 752 Carpenters Way (Tracts 9A and 9B within Area W).
- II. A small-scale map amendment, to change the future land use designation from Residential Medium (RM) to Residential High (RH), and a major modification of PUD zoning to remove a portion of Area U and create a new Tract 12 in Area W for a clubhouse/amenity center to serve future multi-family residential uses on Tracts 4 and 11, in lieu of a previously approved 70,100 sq. ft. assisted living facility, on approximately 2.48 acres located at 503 Carpenters Way (Area U).
- III. A major modification of PUD zoning to increase the maximum number of single-family detached dwelling units from 14 to 17 on approximately 19.02 acres located south of Heatherpoint Drive, east of Carpenters Way, and north of Audubon Oaks Drive (Tract 1 within Area W).
- IV. A major modification of PUD zoning to adopt a new site development plan and reduce the maximum number of single-family detached dwelling units from 46 to 44 on approximately 33.24 acres generally located north of Interstate-4, south of Wedgewood Estates Boulevard, and west of Carpenters Way (Tracts 5, 6, and 7 within Area W).
- V. A major modification of PUD zoning to adopt a new site plan to allow 116 single-family attached (townhome) dwelling units and 700 multi-family dwelling units, in lieu of previously approved entitlements for 834 multi-family dwelling units, on approximately 57.74 acres located north of Lakeland Park Center Drive, east of Carpenters Way, east and west of Audubon Oaks apartments (Tracts 2, 3, 4, 10 and 11 within Area W).

2.2 Subject Property

The subject property consists of the former Wedgewood Golf & Country Club, a privately owned 18-hole golf course which was previously approved for residential development (PUD 5962). This major modification contains changes to all tracts within the PUD except for Tract 8 and proposes to add a new Tract 12 through the conveyance of a 2.48-acre portion of Area U that is currently approved for a 70,100 sq. ft. Assisted Living Facility.

The current PUD zoning, as approved in December 2022, allows for the redevelopment of the former golf course for 834 multi-family dwelling units, 60 single-family attached (townhome) dwelling units and 60 single-family detached dwelling units. Following the 2022 approval, two minor modifications to the PUD zoning were approved by the Planning & Zoning Board. The first minor modification, approved in December 2023, allows for the development of 96 single-family attached (townhome) dwelling units on Tract 2 in lieu of the previously approved 230 multi-family dwelling units as an alternative development option. The second minor modification, approved in July 2024, allows for the development of 162 single-family attached dwelling units and 120 multi-family dwelling on Tracts 4, 10 and 11 in lieu of the previously approved 450 multi-family dwelling units as an alternate development option.

2.3 Project Background

The purpose of the request, through a small-scale land use map amendment and major modification of the PUD zoning, is to adopt a new site development plan which will allow for the development of up to 61 single-family detached dwelling units, 212 single-family attached (townhome) dwelling units, and 700 multi-family dwelling units. A revised master site development plan, which shows the location of proposed building footprints, off-street parking, internal driveways, access points, amenities, and the layout of new single-family subdivisions, is included as Exhibit “W-3.”

2.4 Existing Uses of Adjacent Properties

Boundary	Existing Land Use	FLUM	Zoning	Context
North	Single-Family Residential	RL	PUD	SNH
South	Single-Family Residential/Vacant Commercial/Interstate 4	RL, RAC, N/A	PUD, C-5, N/A	SNH, SCO, N/A
East	Undeveloped Land/Cemetery/Single-Family Residential	RM, IAC	O-3, PUD	SNH, SCO
West	Single-Family Attached/Multi-Family Residential/Public Recreation	RL, RM, RH	PUD	SNH

2.5 Attachments

- Exhibit U-1: Truncated Site Development Plan for Area U
- Exhibit W-1: Legal Description
- Exhibit W-2: Base Map of Subject Property
- Exhibit W-3: Master Site Development Plan
- Exhibit W-3a: Lot Layout Townhouses
- Exhibit W-4a: Landscape Buffer Plan
- Exhibit W-4b: Landscape Buffer Typical Details
- Exhibit W-5a: Roadway Site Circulation Plan
- Exhibit W-5b: Pedestrian Site Circulation Plan
- Exhibits W-6a – W-6f: Roadway/Trail Sections
- Exhibit W-7: Site Data Table & Phasing Plan

3.0 Discussion

The current PUD approval allows for a total of 954 dwelling units. Below is a table which compares the current PUD entitlements with the changes recommended by the Planning & Zoning Board which differ from the original request that was submitted by the applicant.

As originally advertised prior to the September 16, 2025, Planning and Zoning Board hearing, the proposed changes reduced the maximum number of multi-family dwelling units from 834 to 750, but increased the total number of dwelling units allowed approximately 2.5% from 954 to 978 due to: a proposed 44-unit townhome development on Tracts 9A/9B; the inclusion of a new 2.48-acre parcel

(Tract 12); and changes to the lot layout in the single-family development area, Tracts 1,5, 6, and 7 which increased the total number of single-family lots from 60 to 66.

Due to the inability to obtain a motion for approval of the original request at the October 21st meeting of the Planning & Zoning Board, the applicant revised the application in response to the public comments and direction provided by Board members at that meeting. The revised request reduced the maximum number of multi-family dwelling units from 834 to 700 through the removal of 120 multi-family units on Tract 11 and adding 70 additional units to Tract 4 by increasing the maximum building height from three to four stories. On Tracts 9A/9B, the eight townhomes proposed for Tract 9A were eliminated.

For Tract 10, the development tract closest to Fairfield on the Tee, the boundaries were modified to include the portion of Tract 11 which previously allowed 120 multi-family dwelling units and increased the total number of single-family attached dwelling units from 58 to 116. For the single-family portion (Tracts 1, 5, 6, and 7), the applicant offered to donate the entirety of Tract 6 to the City as future right-of-way for the Lakeland Park Center Drive Extension reducing the proposed number of single-family detached dwelling units from 66 to 61. Altogether, the revised application proposes a total of 973 dwelling units: 61 single-family detached dwelling units on Tracts 1, 5, 6 and 7, 212 single-family attached dwelling units on Tracts 8, 9B and 10, and 700 multi-family units on Tracts 2, 4, and 11.

	Current PUD	Proposed Changes
Single-family detached units	60	61
Single-family attached units	60	212
Multi-family units	834	700
Total	954	973

The table above does not include the two minor modifications to the PUD zoning approved in 2023 and 2024 which allowed for the development of up to 258 single-family attached (townhome) dwelling units in lieu of 560 multi-family dwelling units on Tracts 2, 4, 10 and 11 as a development option. Under the proposed changes to the PUD zoning, the two minor modifications will be rescinded and will no longer be a development option going forward. Since the changes to the PUD address multiple development tracts, the current development entitlements and proposed changes for each tract are summarized separately below.

Tract 1

Tract 1 is currently approved for a 13-lot single-family detached subdivision. The proposed changes to Tract 1 provide for four additional single-family lots by reconfiguring the subdivision along a single street which will run east-west and parallel to the Wedgewood Lake Estates subdivision to the north. Lots in Tract 1 will have a minimum lot width of 90 feet and a minimum size of 10,000 sq. ft.

Tract 2

Tract 2 is currently approved for 230 multi-family dwelling units spread throughout seven (7) buildings with a maximum building height of four-stories. The proposed changes to Tract 2 will allow for 240 multi-family dwelling units spread throughout eight (8) buildings with a maximum height of three-stories.

Tract 3

Tract 3 is currently approved for 154 multi-family dwelling units spread throughout four (4), four-story buildings. The proposed changes to Tract 3 will allow 60 multi-family dwelling units in two (2), three-story buildings.

Tracts 4, 10 and 11

Tracts 4, 10 and 11 are currently approved for a maximum of 450 multi-family dwelling units spread throughout (13) buildings with building heights ranging from two to four-stories. As originally advertised, the proposed changes would have allowed 210 multi-family buildings on Tract 4 spread throughout seven (7), three-story buildings, 58 single-family attached (townhome) dwelling units on Tract 10 with a maximum height of two-stories, and 240 multi-family dwelling units on Tract 11 spread throughout eight (8), three-story buildings.

The revised application reduces the number of multi-family dwelling units from 240 to 120 on Tract 11; increases the number of multi-family dwelling units on Tract 4 from 210 to 280 by increasing the maximum height of each building from three to four stores; and increase the proposed number of two-story single-family attached dwelling units on Tract 10 from 58 to 116 through the inclusion of the portion of Tract 11 which was previously proposed for 120 multi-family dwelling units.

Tract 5

Tract 5 is currently approved for a 14-lot single-family detached subdivision. The proposed changes to Tract 5 provide for one additional single-family lot through minor changes to the lot layout. Lots in Tract 5 shall have a minimum lot width of 60 feet and a minimum lot size of 7,500 sq. ft.

Tract 6

Tract 6 is currently approved for a 12-lot single-family detached subdivision. As originally advertised, the proposed changes to Tract 6 would have reduced the number of lots from 12 to 5 to account for the donation of right-of-way for the future extension of Lakeland Park Center Drive. The five-lot subdivision would have been oriented towards Lakeland Park Center Drive with access limited to a one-way spur road. The revised application completely removes the five residential lots in Tract 6 which will be set aside as part of the lands donated to the City as right-of-way for the future extension of Lakeland Park Center Drive.

Tract 7

Tract 7 is currently approved for a 20-lot single-family detached subdivision. The proposed changes to Tract 7 provide for nine additional single-family lots through the reconfiguration of the subdivision and addition of a secondary connection to Lakeland Park Center Drive which will provide another means on ingress and egress for residents of Wedgewood. Lots in Tract 7 will have a minimum lot width of 60 feet and a minimum lot size of 7,500 sq. ft.

Tract 8

Tract 8 is currently approved for 60 single-family attached dwelling units. No changes are proposed to Tract 8 with this request.

Tract 9A and 9B

Tracts 9A and 9B are identified as a future development area in the approved PUD. As originally advertised, the proposed changes would have allowed for a total of 44 single-family attached (townhome) dwelling units on Tracts 9A and 9B with a maximum height of two stories. Eight of the townhomes would have been located in Tract 9A, north of Wedgewood Estates Boulevard, which will

be reconstructed to align with Heather Drive, with the remaining 36 townhome units located on Tract 9B, south of Wedgewood Estates Boulevard.

The revised application eliminated the eight townhome units proposed for Tract 9A which will now be donated by the developer for use as right-of-way to assist with the reconstruction and alignment of Wedgewood Estates Boulevard with Heatherpoint Drive. The remaining 36 townhome units on Tract 9B, south of Wedgewood Estates Boulevard, necessitate the small-scale map amendment to change the future land use designation from Residential Low (RL) to Residential Medium (RM). The RL future land use designation allows for a maximum density of five (5) dwelling units per acre while the RM future land use will allow for a maximum density of 12 dwelling units per acre. The 36 townhomes will be located about the rear yard of existing single-family homes located to the south and east along Staffordshire Drive.

Tract 12

The applicant proposes to incorporate a vacant 2.48-acre parcel located at 503 Carpenters Way. The subject property, which is located to the south of the Savannah Cottage Adult Congregate Living Facility, is currently zoned as part of Area U within the parent PUD for Carpenters Way (Ordinance 4773, as amended). Under the current PUD zoning, the parcel has entitlements which would allow for the development of a two-story, 70,100 sq. ft. assisted living facility. Through this modification 1.94 acres of the subject property will be used to create a new Tract 12 which will be used to develop an amenity center in support of adjacent multi-family residential uses on Tracts 4 and 11. The remaining 0.54 acres will be incorporated into the site development plan for Tract 4. To allow for this change, the applicant is requesting a small-scale map amendment to change the future land use designation on the 2.48-acre parcel from Residential Medium (RM) to Residential High (RH). This change aligns with the future land use designation on Tracts 4 and 11.

Project Phasing

According to the applicant, the project will be developed through two separate development phases. Phase I will consist of 460 multi-family dwelling units in Tracts 3, 4, 11 and the amenity center in 12. Phase II will consist of 61 single-family detached dwelling units in Tracts 1, 5, and 7, 240 multi-family units in Tract 2, and 212 single-family attached dwelling units in Tracts 8, 9 and 10.

Future Land Use Map Amendments

As previously discussed, the applicant is requesting a small-scale map to change the future land use designation from Residential Low (RL) to Residential Medium (RM) on 8.48 acres and from Residential Medium (RM) to Residential High (RH) on 2.48 acres. The surrounding future land use districts for Tracts 9A and 9B and the proposed Tract 12 are summarized in the following two tables.

Surrounding Land Uses for Tracts 9A and 9B	
North	RM (Residential Medium)
South	RL (Residential Low)
East	RM (Residential Medium) and RL (Residential Low)
West	RM (Residential Medium) and RH (Residential High)

Surrounding Land Uses for Tract 12	
North	RH (Residential High)
South	RL (Residential Low)
East	RH (Residential High)
West	RM (Residential Medium)

Utilities

During the Concept Plan Review in December 2021, the Water Utilities Division expressed concerns regarding the ability of the Wedgewood lift station to accept additional flows. The Water Utilities department performed a hydraulic evaluation to determine if there is sufficient capacity in the City's existing water distribution and wastewater collection systems to accept the additional flows that will be created with this development. Based on this analysis, it was determined that in addition to new water and wastewater lines within their project site, required wastewater improvements include a new force main across Interstate 4 to State Road 33 (Lakeland Hills Boulevard) as well as the construction of a new lift station with generator to replace the existing Wedgewood station. Water will be handled by existing offsite infrastructure. As such, each phase of the project shall be required to complete a Capacity Application to determine whether there is capacity in the system to handle the proposed phase of development.

Since 2022, staff has received and reviewed a site plan for a wastewater force main that would run from Wedgewood to Lake Crago Drive via a route beneath Interstate 4, along North Florida Avenue and Glenwood Drive to Lakeland Hills Boulevard. However, no plans have been approved as the master utility plan has been amended several times. An infrastructure agreement with Water Utilities has not been approved by the City Commission to-date.

School Concurrency

In April 2022, the applicant requested a non-binding determination of school concurrency from the Polk County School Board (PCSB). At that time, there was adequate capacity available at the zoned middle school (Sleepy Hill Middle School) and high school (Lake Gibson Senior High School). Both the zoned elementary school (North Lakeland Elementary) and the adjacent elementary school (Sleepy Hill Elementary) have partial capacity available which when combined would be sufficient to support the proposed development.

As part of the proposed modification to the PUD, the applicant has submitted another request to the PCSB for a non-binding determination of school concurrency. The non-bonding request shows capacity at both the zoned elementary school (North Lakeland) as well as the adjacent elementary school (Edgar Padgett). There is partial capacity in the zoned middle school (Sleepy Hill) with more capacity available in the adjacent middle school (Lake Gibson). Both the zoned high school (Lake Gibson) and the adjacent high school (Tenoroc) have capacity. Since this determination is non-binding, the applicant will be required to submit a binding concurrency application to the School Board at the time of site plan review before any of the development phases can move forward.

Public Comment

Prior to the public hearing on September 16, 2025, staff received five e-mails and two phone calls from concerned residents. Issues raised included traffic impacts, stormwater management challenges, loss of green space related to the proposed development of Tracts 9A and 9B, removal of the roundabout where Wedgewood Estates Boulevard will be realigned to intersect Carpenters Way at Heatherpoint Drive, the loss of green space and removal of a 75-year old tree where a proposed street between Tracts 5 and 7 intersects Derby Drive, and opposition to having new single-family lots which will abut the rear yard of existing single-family homes on Derby Drive.

Existing PUD transportation conditions include right-of-way dedication for the Lakeland Park Center (LPC) Drive Extension, right-of-way dedication and construction of a roundabout at a realigned Carpenters Way/Wedgewood Estates Boulevard/Heatherpoint Drive intersection, construction of a transit shelter, an updated major traffic study for all project phases for which a building permit has not been requested as of the writing of this recommendation, and bicycle/pedestrian infrastructure requirements including the sidewalk and trail connections utilizing the former golf cart paths.

Concerns related to stormwater management cited existing flooding problems created by the poor condition of existing drainage infrastructure and the potential for increased flooding with development on the former golf course property. Each site and subdivision construction plan submittal will require stormwater review by the Southwest Florida Water Management District (SWFWMD) as well as the City of Lakeland Public Works Department as a member of the City's Development Review Team. The City's engineering consultant for the LPC Drive Extension continues to work with the developer's engineer to coordinate pond and floodplain compensation needs for both the road and development during the formulation of a proposed transportation Development Agreement and in advance of permitting.

At the time of adoption of the current PUD approval, Tracts 9A and 9B were identified as "Future Development" due to the lack of any pending development proposal. While no entitlements were specified for either tract, they were never intended to be set aside as a green space for the residents of Wedgewood. The new street between Tracts 5 and 7, which will intersect Derby Drive, was included on the master site development plan in the 2022 PUD approval. While the single-family lots in Tract 7 were reconfigured, increasing the number of lots that will back up to existing homes on Derby Drive from eight (8) to seventeen (17), the single-family lots in Tract 6, which would have abutted the rear yards of homes located at the south end of Derby Drive, have been removed as previously noted for donation as right-of-way for the future extension of Lakeland Park Center Drive.

Additional comments received after the public hearing on September 16th, include the cost of providing police, fire, and EMS services for the proposed development as well as additional buses and teachers needed for school-age children. A new fire station, Station No. 8 located at the Sleepy Hill Road/Mall Hill Road intersection is under design and scheduled for construction to begin in Spring 2026. Construction is expected to be completed in Spring 2027. The new Polk County EMS Station on Robson Street is also under design. Using the Staffordshire/Carpenters Way intersection as a good central point in the development, Fire Station No. 8 is located 1.6 miles away and the Robson EMS Station is 1.5 miles away.

Regarding school concurrency, city staff requires the submittal of non-binding concurrency determinations from the School District at the time of zoning approval and binding determinations at the time of site plan or subdivision submittals. City staff provides Certificates of Concurrency to the School District once the site plan is approved and when a subdivision plat is recorded, further coordinating with School District staff on multi-phased developments to ensure that appropriate number of completed and remaining units are tracked. In 2025, the School District commenced a

process to rezone high schools throughout Polk County to balance attendance zones; the rezoning of middle and elementary schools is expected to continue through Spring 2027. School District coordinates meetings with local planning agencies to review proposed developments and school capital projects. Such coordination meetings with City of Lakeland are contained in the Certification Agreement with the Florida Department of Commerce.

As previously stated, the applicant was unable to secure a motion for approval at the October 21, 2025, Planning & Zoning Board meeting due to Board member concerns related to compatibility of the three-story multi-family uses in Tract 11 with the two-story condos located in Fairfield on the Tee. A motion was passed to continue the application until the following month's meeting on November 18th with the Board suggesting that buildings 11A, 11B and 11C be removed and that the landscape buffer planned along the southern side of Fairfield on the Tee be increased in size and additional plantings added.

In response to the direction given at the October meeting, the applicant reduced the maximum number of multi-family dwellings units on Tract 11 from 240 to 120. The portion of Tract 11 located to the south of Fairfield on Tee, previously proposed for three apartment buildings, has now been added to Tract 10 and proposed for 58 additional single-family townhome units increasing the total number of townhomes on Tract 10 from 58 to 116. Lastly, to account for the removal of multi-family units on Tract 11, the applicant is proposing to increase the maximum number of multi-family dwelling units on Tract 4 from 210 to 280 through the increase of the maximum building height from three-stories to four-stories.

In response to suggestions made by Planning & Zoning Board members at the October meeting, the applicant held a community meeting on November 13, 2025, at the Larry R. Jackson Branch library. Approximately 15 citizens attended and participated in discussions regarding the proposed plan changes outlined above, including traffic, compatibility of adjacent proposed residential units, flooding and maintenance of the existing and proposed stormwater ponds.

3.1 Transportation and Concurrency

The City Commission approved PUD Ordinance No. 5962 on December 5, 2022, with a total count of 954 dwelling units, 40 multi-family units less than the 994 units recommended for approval by the Planning & Zoning Board, in response to public comments about building height and setback compatibility received from residents of Sandwedge Villas and Fairfield on the Tee.

The current land use and zoning request increases the total unit count from 954 to 973 units. Based on average rates published in the Institute of Transportation Engineers *Trip Generation Manual* (12th Edition), the new mix of residential units and 19-unit count increase is expected to generate an additional 176 Daily and 8 PM Peak Hour (of Adjacent Street Traffic, 4 PM – 6PM) trips. Subsequent to the original PUD approval in 2022, the professional engineering teams for the developer and City have continued to coordinate the amount and alignment of the right of way needed to accommodate the LPC Drive Extension, to minimize the road's impact on the adjacent Florida Gas Transmission line easement, and to coordinate stormwater and floodplain impacts of both the road and development. The City has purchased approximately 1.32 acres immediately west of the subject property for the LPC Drive extension and has coordinated with the developer's utility engineer to ensure that the planned wastewater force main design does not conflict with roadway design. The Lakeland Area Mass Transit District (Citrus Connection) has re-aligned the inbound portion of its Pink Line to utilize the southbound direction of Lakeland Park Drive Extension (opened in 2022) to support existing and future residents in the vicinity of the Wedgewood community.

The current PUD modification request adds lands within a new “Tract 12” that moves the eastern driveway westward to align with the proposed connection to Tract 5, further away from the arch and adjacent curve on Carpenters Way that created sight distance limitations for motorists as noted by staff during its review of the site plan. The modified PUD will also include a more gradual curve along the Wedgewood Estates Boulevard realignment approaching Carpenters Way, complying with Florida Greenbook design standards, and a more direct connection from Tract 7 to the LPC Drive Extension to better distribute project traffic between Carpenters Way, Wedgewood Estates Boulevard and the future LPC Drive Extension.

In 2023, the developer submitted a site plan (Project No. SIT23-020) for 330 multi-family units in Tracts 10, 11 and a portion of Tract 4, which was not approved due to the existing PUD condition that requires the execution and recording of a Development Agreement addressing the timing of the transportation mitigation requirements also contained within the PUD. Public hearings for the required Development Agreement are being scheduled for the December 1st and 15th City Commission meetings, concurrent with the land use changes and PUD modification that were ultimately approved by the Planning & Zoning Board on November 18th. In addition to the timing of meeting the required transportation conditions, the Development Agreement will seek to establish City Multi-Modal Transportation Impact Fee pre-payment and credit provisions, a proportionate-share of net payments required of the developer, and impact fee collections that could be invested in projects such as the LPC Drive Extension for which construction is not currently funded in the City’s Capital Improvement Plan. For the realigned Wedgewood Estates Boulevard intersection opposite Heatherpoint Drive, staff recommended a revision to the PUD condition that currently requires right of way dedication and construction of a roundabout at this location to include the option of a traffic signal if subsequent analyses determine comparable operations with a lower cost and reduced maintenance of traffic impacts to adjacent residents during construction. Roundabouts typically require extended lane and intersection approach closures that are not generally expected with traffic signal and associated turn lane installation.

Land Use Transportation Evaluation

Transportation concurrency entitlements are not granted through land use approvals, which allow a broad range of development types. City staff, however, analyzed the potential trip generation impacts associated with changing the Residential Low (RL) land use on 8.48 acres within Tracts 9A and 9B to Residential Medium (RM), assuming 42 single-family detached homes could be accommodated on the property under the current RL land use and that the RM land use could accommodate 102 low-rise multi-family apartments. Using data published in the ITE *Trip Generation Manual* (12th Edition) for Land Use Codes 210 (Single-Family Detached) and 220 (Multi-Family Low-Rise), the land use change could generate an estimated 14 additional PM Peak Hour Trips (of Adjacent Street Traffic, 4 PM - 6 PM). The requested land use change from Residential Medium (RM) to Residential High (RH) on 2.48 acres within Tract 12 could generate an additional 32 PM Peak Hour Trips assuming that 30 low-rise multi-family units are currently allowed and could be replaced with 186 high-rise multi-family units (Land Use Code 222). The cumulative potential traffic increase associated with the two requested land use changes total 46 P.M. Peak Hour Trips, although the final PUD zoning ordinance will control the ultimate number and type of dwelling units on the subject properties and will form the basis of the concurrency entitlements that will be granted through the Development Agreement to be considered by the City Commission.

3.2 Comprehensive Plan Compliance

The Community and Economic Development Department reviewed this request for compliance with Lakeland Comprehensive Plan: Our Community 2030 and it is our opinion that the request is consistent with the Comprehensive Plan.

4.0 Recommendation

4.1 Community and Economic Development Staff

The Community and Economic Development Department reviewed this request and recommends approval of the small-scale land use map amendments and the major modification to PUD zoning. Letters of notification were mailed to four hundred and eight (408) property owners within 500 feet of the subject property. Objections to the request were primarily related to the number of dwelling units, compatibility concerns about the single-family attached, multi-family apartment buildings, increased traffic, increased public safety needs (Police, EMS, Fire), increased numbers of students in already crowded schools, additional school funding needed for the increase in the number of students, lack of maintenance of the former golf course property, and stormwater retention and drainage.

4.2 The Planning & Zoning Board

The recommendation was approved by a 5—1 vote of the Board.

It is recommended that the request for a change in Future Land Use from Residential Low (RL) to Residential Medium (RM) on 8.48 acres, a change in Future Land Use from Residential Medium (RM) to Residential High (RH) on 2.48 acres and a major modification to the PUD zoning, as described above and in Exhibits “U-1,” “W-1,” “W-2,” “W-3,” “W-3a,” “W-4a,” “W-4b,” “W-5a,” “W-5b,” “W-6a,” “W-6b,” “W-6c,” “W-6d,” “W-6e,” “W-6f” and “W-7” be approved subject to the following conditions:

Area U and W in Ordinance 4773, as amended

XXII. Area U

- A. Land Use Intensity: PUD-49
- B. Permitted Uses: Assisted living facility in one or more buildings.
- C. Maximum Floor Area:
 - Phase I: 21,100 square feet.
 - Phase II: ~~70,100 square feet~~
- D. Maximum Building Height:
 - Phase 1: One (1) story
 - Phase 2: ~~Two (2) stories~~
- E. Setbacks
 - 1. From Carpenters Way: Fifty (50) feet.
 - 2. From all other project boundaries; Twenty-five (25) feet.
- F. Site Development Plan:

The site shall be developed in accordance with the site development plan included as Exhibit "U-1"

- G. Requirements for parking, landscaping, buffering etc. shall be in accordance with City of Lakeland Land Development Regulations. This shall include at a minimum the installation of a hedge and canopy trees on the ratio of one tree per fifty (50) linear feet along Carpenters Way.

XIV. Area W

A. Permitted Uses.

1. Single-Family Detached Residential Uses: Tracts 1, 5, ~~6~~, and 7
- ~~2. Single-Family Attached Residential Uses: Tracts 2*, 4, 8, 9, and 10 and 11**~~
2. Single-Family Attached Residential Uses: Tracts 8, 9B, and 10
- ~~3. Multi-Family Residential Uses: Tracts 2*, 3, 4, 10 and 11**~~
3. Multi-Family Residential Uses: Tracts 2, 3, 4 and 11
- ~~4. Future Development Tract: Tract 9~~

~~*As an alternative development plan in lieu of 230 multi-family dwelling units, Tract 2 may be developed for single-family attached dwelling units in accordance with the maximum intensity of use specified in Condition B. 2. and Exhibit "W 3a." Prior to the issuance of a building permit, written notice shall be provided to the Community and Economic Development Department in which the developer elects to develop Tract 2 for one of the two approved development plans. When the written notice of election has been delivered, the development plan that was not chosen will no longer be an approved use for the property.~~

~~**As an alternative development plan in lieu of 330 240 multi-family dwelling units, Tracts 4, 10 and 11 may be developed for single-family attached dwelling units in accordance with the maximum intensity of use specified in Condition B. 2. and Exhibit "W 3b." Prior to the issuance of a building permit, written notice shall be provided to the Community and Economic Development Department in which the developer elects to develop Buildings 4D, 4E, 4F and 4G and Tracts 10 and 11 for one of the two approved development plans. When the written notice of election has been delivered, the development plan that was not chosen will no longer be an approved use for the property.~~

4. Amenity Center for Multi-Family Residential Uses: Tract 12

5. Right-of-way Donations: Tract 6 and 9A

- B. Maximum Intensity of Use: The maximum intensity of use for each development tract shall be as follows.
1. Single-Family Detached Residential Uses (Tracts 1, 5, ~~6~~ and 7):
 - a. Tract 1: ~~44-17~~ single-family detached dwelling units
 - b. Tract 5: ~~44-15~~ single-family detached dwelling units

- ~~e. Tract 6: 12 single-family detached dwelling units~~
- ~~d. Tract 7: 20 single-family detached dwelling units~~
- c. Tract 7: 29 single-family detached dwelling units
- 2. Single-Family Attached Residential Uses (Tracts 2, 4, 8, 9B, and 10 and 11)
 - ~~a. Tract 2: 96 single-family attached dwelling units~~
 - ~~b. Tract 8: 60 single-family attached dwelling units~~
 - ~~c. Tracts 4, 10 and 11: 162 single-family attached dwelling units~~
 - a. Tract 8: 60 single-family attached dwelling units
 - b. Tract 9B: 36 single-family attached dwelling units
 - c. Tract 10: 116 single-family attached dwelling units
- 3. Multi-family Residential Uses (Tracts 2, 3, 4, ~~10 and 11~~)
 - a. Tract 2: ~~230~~ 240 multi-family dwelling units
 - b. Tract 3: ~~454~~ 60 multi-family dwelling units
 - ~~c. Tracts 4/10/11: 450 multi-family dwelling units~~
 - c. Tract 4: 280 multi-family dwelling units
 - d. Tract 11: 120 multi-family dwelling units
- 4. Transfer of Units Between Development Tracts: Residential units may be transferred between development tracts with the same residential use type (i.e. multi-family residential to multi-family residential or single-family detached residential to single-family detached residential). A maximum five percent (5%) increase in residential units may be permitted above the amount approved in any receiving tract provided that the total cumulative residential density for the development does not exceed 973 dwelling units. Any redistribution of units within this limit shall not require a formal amendment to the approved PUD or Development Agreement but will be subject to staff-level review and approval for consistency with the overall master plan and available infrastructure capacity.

C. Development Standards:

1. Single-Family Detached Residential Uses:

- a. Tract 1: In accordance with the RA-3/SNH context sub-district except as follows
 - i. Minimum Lot Width: 90 feet
 - ii. Minimum Lot Area: 10,000 sq. ft.
 - iii. Minimum Front Setback: 25 feet
 - iv. Minimum Street Side Setback: 15 feet
 - v. Minimum Side Setback: 7.5 feet

- vi. Minimum Rear Yard Setback: 25 feet
- b. Tracts 5, 6-and 7: In accordance with the RA-3/SNH context sub-district except as follows
 - i. Minimum Lot Width: 60 feet
 - ii. Minimum Lot Area: 7,500 sq. ft.
 - iii. Minimum Front Setback: 25 feet
 - iv. Minimum Street Side Setback: 15 feet
 - v. Minimum Side Setback: 7.5 feet
 - vi. Minimum Rear Yard Setback: 25 feet
- 2. Single-Family Attached Residential Uses: In accordance with the MF-12/SNH context sub-district except as follows.
 - a. Minimum Setbacks for Townhouse Building Groups
 - i. From Property Lines of Adjacent Multi-Family Uses: 25 feet.
 - ii. Minimum Internal Setback Between Building Groups: 10 feet.
 - b. Development Standards for Single-Family Attached Dwellings:
 - i. Minimum Lot Width: 19 feet
 - ii. Minimum Living Area: 1,463 sq. ft.
 - iii. Minimum Front Setback: ~~25~~20 feet
 - iv. Minimum Street Side Setback: 15 feet
 - v. Minimum Interior Side Setback 0 feet
 - vi. Minimum Rear Yard Setback: 10 feet
 - vii. Maximum Building Height: Two (2) stories.
 - viii. Maximum number of dwellings per group: Six (6)
 - c. Buffering for Single-Family Attached Dwellings Located South of Fairfield on the Tee: An enhanced buffer is required between existing townhomes in Fairfield on the Tee and new single-family attached dwellings located in Tract 10. To provide a minimum separation distance of 40 feet between structures, and 20 feet from rear lot lines to rear lot lines, a Type "B" landscape buffer as shown in Exhibit W-4b, with a minimum width of 20 feet, shall be installed along the southern property line of Fairfield on the Tee.
- 3. Multi-Family Residential Uses: In accordance with the MF-12/SNH context sub-district except as follows.
 - a. Minimum Setbacks:
 - i. From Property Lines of Adjacent Multi-Family Uses: 25 feet.
 - ii. Minimum Internal Setback Between Buildings: 15 feet

b. Maximum Building Height: ~~Four (4)~~ Three (3) stories**

c. Architectural Design: At least 75% of the units above the first floor of each building shall have a balcony. Multi-family buildings shall reference the architectural elements of existing multi-family uses located within the Carpenters Way PUD, including but not limited to, roof type, finishing materials, and paint colors used.

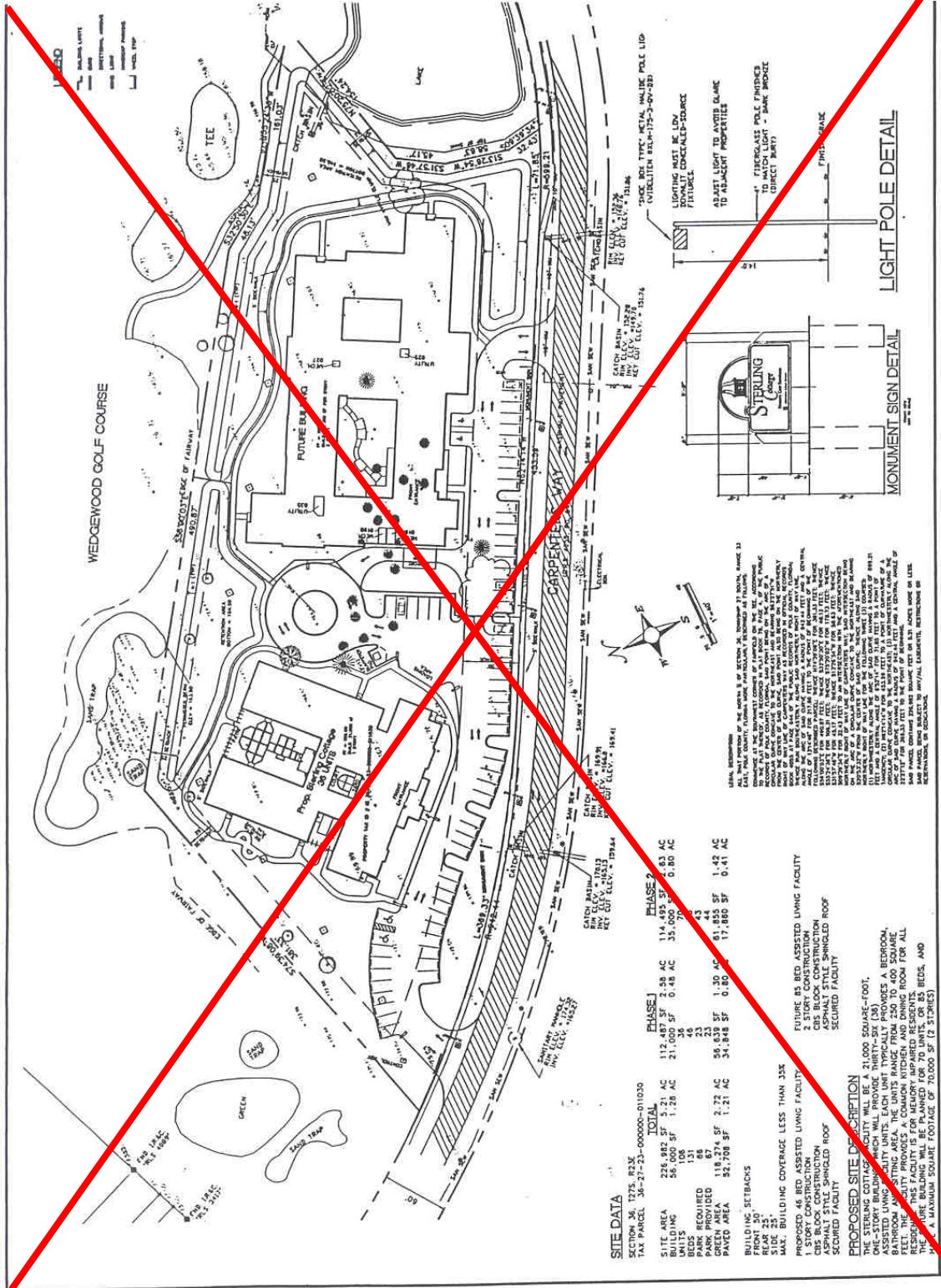
**Four-story buildings shall be limited to the specific buildings listed in the Site Data Table and the locations shown on the Master Site Plan, Exhibit "W-3," and the Site Data Table & Phasing Plan, Exhibit "W-7." All other multi-family buildings shall be limited to a maximum height of ~~either two (2) or three (3)~~ stories as specified in Exhibit "W-7".

- D. Site Development Plan: The site shall be developed in accordance with the proposed site development plan included as Exhibits "W-3", "W-3a" and "W-3b" and the site data table and phasing plan included as Exhibit "W-7." With the approval of the Director of Community & Economic Development, minor alterations may be made at the time of site plan review without requiring a modification to the PUD zoning.
- E. Landscaping and Buffering: In accordance with Section 4.5 of the Land Development Code, the Landscape Buffer Plan, Exhibit "W-4a" and Typical Buffer Details, Exhibit "W-4b." In addition to the buffering shown on the Landscape Buffer Plan, a Type "D" buffer as shown in Exhibit "W-4b" shall be installed and maintained adjacent to the north and west boundaries of the United Brotherhood of Carpenters and Joiners of America cemetery. Within Tract 8, the buffer shall include a six-foot tall vinyl privacy fence. Within Tract 2, the buffer shall include either a black or green vinyl chain link fence or decorative metal picket fencing with a minimum height of four feet.
- F. Access
1. Access from the development tracts onto Carpenter's Way shall be limited to the intersections illustrated in Exhibit "W-3"
 2. Access improvements on Carpenters Way at street or driveway connections to the development tracts, such as turning lanes, shall be constructed in accordance with the standards and specifications of the Florida Department of Transportation and City as appropriate and determined during the permitting process. The cost of such improvements shall be the responsibility of the applicant.
 3. All roads within the development shall be constructed in accordance with City of Lakeland standards.
 4. Access to the United Brotherhood of Carpenters and Joiners of America cemetery shall be formalized through a recorded agreement. If such agreement already exists, a copy of the agreement shall be provided prior to the issuance of the first building permit for the development.
- G. Civic Open Space and Recreational Amenities
1. A development-wide paved multi-use trail system shall be constructed as illustrated in Exhibits "W-5b," "W-6a," "W-6b," "W-6c," ~~and "W-6d,"~~ "W-6e," and "W-6f."

2. Tracts 2 and 3 shall have their own amenity center with a clubhouse, swimming pool and other recreational facilities.
 3. Tracts 4, 10 and 11 shall have a master amenity center consisting of a clubhouse, swimming pool and other recreational facilities that is shared by the residents of all three tracts. This master amenity center shall be located on Tract 12.
- H. Off-street Parking: Off-street parking shall be provided in accordance with Section 4.11 and Table 4.11-1 of the Land Development Code. If the minimum amount of parking cannot be provided for a specific development tract, the maximum number of multi-family dwelling units shall be reduced to align with amount of parking provided.
- I. Signage: In accordance with Section 4.9 of the Land Development Code.
- J. Outdoor Lighting: Except for City-approved streetlights within rights-of-way, all outdoor lighting fixtures shall be of the “fully shielded” type such that the light emitting, distributing, reflecting, and refracting components of the light fixture (lamp, lens, reflective surfaces, etc.) shall not extend beyond the opaque housing of the fixture.
- K. Stormwater Ponds: stormwater ponds shall be designed as water features with a natural appearance and shape. The use of fencing shall be minimized, but in the event fencing is required, fence materials used shall consist of either black or green vinyl chain link or decorative metal picket fencing. Any landscaping shall be located on the outside of such fencing.
- L. Transportation:
1. Prior to the first site plan or preliminary plat submittal, the developer and City shall execute and record in the Official Records of the Polk County Clerk of Courts a Development Agreement through which the timing of the following specific transportation concurrency mitigation requirements are addressed. This agreement shall be executed by the City Commission and recorded with the Polk County Clerk of Courts.
 - a. Right-of-way dedication to the City of Lakeland to accommodate the Lakeland Park Center Drive Extension south of Carpenters Way. The dedicated right-of-way and design shall minimize impacts to an existing gas transmission line and accommodate necessary stormwater treatment facilities for this future roadway or shared facilities for the roadway and adjacent development.
 - b. Right-of-way dedication and construction of a roundabout or traffic signal with turn lanes at the Carpenters Way/Wedgewood Estates Boulevard/Heatherpoint Drive intersection, including the realignment of Wedgewood Estates Boulevard to align with Heatherpoint Drive as generally illustrated in Exhibit “W-5a”.
 - c. Installation of a transit shelter and bus bay shall be installed on Lakeland Park Drive at the entrance to Tracts 3A/3B to serve a re-alignment of the Citrus Connection’s Pink Line, which is anticipated to occur once the apartment development phases commence.

2. A major update to the Wedgewood Redevelopment Traffic Impact Study (dated March 2022) shall be conducted for all remaining project phases for which a building permit has not been requested by ~~September 1, 2027~~ November 1, 2035.
 3. Bicycle/Pedestrian Infrastructure
 - a. An integrated multi-use trail network shall be implemented throughout the proposed development area as depicted in Exhibits “W-5a” and “W-5b”. The appropriate cross-sections for each trail and street type shall be included with site plan and subdivision construction plan submittals for the applicable project phases. Raised crosswalks shall be installed at each location where this trail network crosses Carpenters Way.
 - b. Concurrent with the construction of the Carpenters Way/Wedgewood Estates Boulevard-Heatherpoint Drive ~~roundabout intersection~~ realignment, a sidewalk shall be constructed along the south side of Carpenters Way to the eastern terminus of the existing frontage sidewalk at Douglas Cook Park.
 - c. ADA-compliant sidewalk routes shall be constructed between each principal multi-family residential building and amenity to the planned trail network and existing frontage sidewalks on the adjacent street network.
 - d. Bicycle parking for each multi-family residential building and amenity shall be provided in compliance with Section 4.11.6 of the Land Development Code, with designs in compliance with Index 900 of the City Engineering Standards Manual.
 4. Prior to the first Certificate of Occupancy for Tract 2, a roadway “stub-out” shall be constructed to the eastern site boundary in support of a future roadway connection to Artega Drive.
 5. Current and future developers within this area of the PUD shall work with the City to support its efforts to locate a high-speed rail (such as Brightline) stop in its vicinity.
- M. Maintenance of Trails, Stormwater Infrastructure and Common Areas: Each development tract shall have a Homeowners Association, Property Owners Association, or some other legal entity which shall be responsible for the perpetual maintenance of trails and common areas within the boundaries of the development tract.

Exhibit "U 1"



SITE DATA

SECTION 36, T12S, R12E
 TAX PARCEL 36-27-23-000000-01030
 TOTAL
 SITE AREA 112,487 SF 2.21 AC
 UNIT 21,000 SF 0.48 AC
 BEDS 46
 BATHS 131
 PARK PROVIDED 67
 GREEN AREA 118,274 SF 2.22 AC
 PAVED AREA 52,798 SF 1.21 AC

PHASE 1	PHASE 2
114,495 SF 2.63 AC	114,495 SF 2.63 AC
35,700 SF 0.80 AC	35,700 SF 0.80 AC
46	46
131	131
67	67
23	23
59,639 SF 1.30 AC	59,639 SF 1.30 AC
34,948 SF 0.80 AC	34,948 SF 0.80 AC
61,855 SF 1.42 AC	61,855 SF 1.42 AC
17,800 SF 0.41 AC	17,800 SF 0.41 AC

PROPOSED SITE DESCRIPTION

THE STERLING COTTAGE FACILITY WILL BE A 21,000 SQUARE-FOOT, ONE-STORY BUILDING WHICH WILL PROVIDE THIRTY-SIX (36) UNITS. EACH UNIT WILL PROVIDE A BATHROOM AND BATHING AREA. THE UNITS RANGE FROM 250 TO 400 SQUARE FEET. THE FACILITY PROVIDES A COMMON KITCHEN AND DINING ROOM FOR ALL UNITS. THE BUILDING WILL BE PLANNED FOR 70 UNITS, OR 83 BEDS, AND A MAXIMUM SQUARE FOOTAGE OF 70,000 SF (2 STORIES).

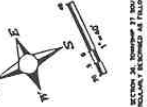
PROPOSED 48 BED ASSISTED LIVING FACILITY

THE 48 BED ASSISTED LIVING FACILITY WILL BE A 114,495 SQUARE-FOOT, ONE-STORY BUILDING WHICH WILL PROVIDE 48 UNITS. EACH UNIT WILL PROVIDE A BATHROOM AND BATHING AREA. THE UNITS RANGE FROM 250 TO 400 SQUARE FEET. THE FACILITY PROVIDES A COMMON KITCHEN AND DINING ROOM FOR ALL UNITS. THE BUILDING WILL BE PLANNED FOR 70 UNITS, OR 83 BEDS, AND A MAXIMUM SQUARE FOOTAGE OF 70,000 SF (2 STORIES).

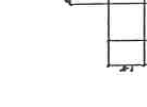
LEGEND

- BUILDING FOOTPRINT
- DRIVE
- SIDEWALK
- LANDSCAPE
- FUTURE BUILDING
- FUTURE DRIVE
- FUTURE SIDEWALK

MONUMENT SIGN DETAIL



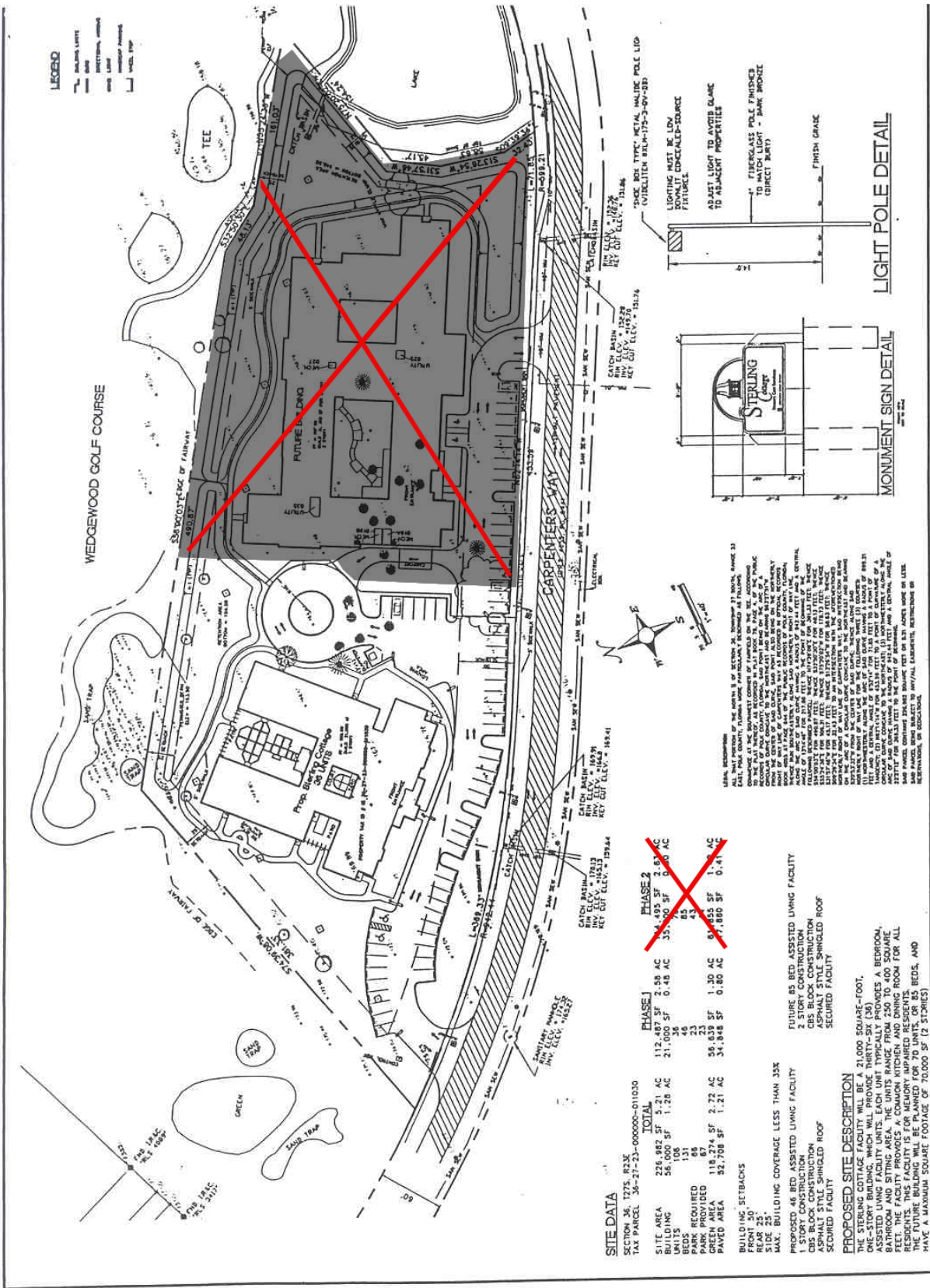
LIGHT POLE DETAIL



LIBRARY DESCRIPTION

ALL LIBRARY BOOKS ARE TO BE STORED IN ACCORDANCE WITH THE NATIONAL ARCHIVES AND RECORDS ADMINISTRATION. THE LIBRARY WILL BE A 21,000 SQUARE-FOOT, ONE-STORY BUILDING WHICH WILL PROVIDE THIRTY-SIX (36) UNITS. EACH UNIT WILL PROVIDE A BATHROOM AND BATHING AREA. THE UNITS RANGE FROM 250 TO 400 SQUARE FEET. THE FACILITY PROVIDES A COMMON KITCHEN AND DINING ROOM FOR ALL UNITS. THE BUILDING WILL BE PLANNED FOR 70 UNITS, OR 83 BEDS, AND A MAXIMUM SQUARE FOOTAGE OF 70,000 SF (2 STORIES).

Exhibit "U-1"



LEGEND

- ▬ BUILDING FOOTPRINT
- ▬ DRIVE
- ▬ SIDEWALK
- ▬ PARKING SPACE
- ▬ LANDSCAPE
- ▬ FUTURE PLANNING
- ▬ TREE

SITE DATA

SECTION 36, T22S, R23W
 TAX PARCEL 36-27-23-00000-01030

	PHASE 1	PHASE 2	TOTAL
SITE AREA	112,487 SF 2.58 AC	14,495 SF 0.33 AC	126,982 SF 2.91 AC
BUILDING	21,000 SF 0.48 AC	35,700 SF 0.80 AC	56,700 SF 1.28 AC
BEYS	36	45	81
PARK REQUIRED	48	43	91
GREEN AREA	59,639 SF 1.30 AC	6,855 SF 0.16 AC	66,494 SF 1.46 AC
PAVED AREA	34,848 SF 0.80 AC	7,780 SF 0.17 AC	42,628 SF 0.97 AC

BUILDING SETBACKS

FRONT 25'
 REAR 25'
 SIDE 25'
 MAX. BUILDING COVERAGE LESS THAN 30%

PROPOSED 48 BED ASSISTED LIVING FACILITY
 CBS BLOCK CONSTRUCTION
 ASPHALT STYLE SHINGLED ROOF
 SECURED FACILITY

PROPOSED SITE DESCRIPTION

THE STERLING COTTAGE FACILITY WILL BE A 21,000 SQUARE-FOOT, ONE-STORY BUILDING, WHICH WILL PROVIDE THIRTY-SIX (36) ASSISTED LIVING FACILITY UNITS. EACH UNIT WILL INCLUDE A BATHROOM, KITCHEN, LIVING AREA, AND BEDROOM. THE FACILITY WILL PROVIDE A COMMON HALLWAY FROM 250 TO 400 SQUARE FEET. THE FACILITY PROVIDES A COMMON KITCHEN AND DINING ROOM FOR ALL RESIDENTS. THIS FACILITY IS FOR MEMORY IMPAIRED RESIDENTS. THE FUTURE BUILDING WILL BE PLANNED FOR 30 UNITS. THE FUTURE BUILDING WILL BE PLANNED FOR 30 UNITS. THE FUTURE BUILDING WILL BE PLANNED FOR 30 UNITS. THE FUTURE BUILDING WILL BE PLANNED FOR 30 UNITS.

NOTE: THE PROPOSED FACILITY IS LOCATED ON THE WEST SIDE OF SECTION 36, TOWNSHIP 22 NORTH, RANGE 23 WEST, COUNTY OF ALTA COUNTY, COLORADO AND IS SUBJECT TO THE ZONING ORDINANCE OF ALTA COUNTY, COLORADO. THE PROPOSED FACILITY IS SUBJECT TO THE ZONING ORDINANCE OF ALTA COUNTY, COLORADO. THE PROPOSED FACILITY IS SUBJECT TO THE ZONING ORDINANCE OF ALTA COUNTY, COLORADO. THE PROPOSED FACILITY IS SUBJECT TO THE ZONING ORDINANCE OF ALTA COUNTY, COLORADO.

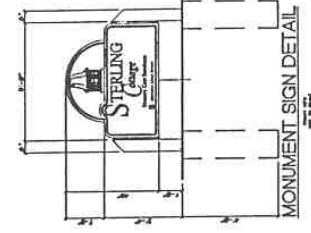
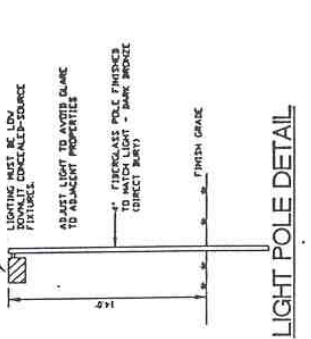


Exhibit "W-1"

Legal Description

Tract 1

Beginning at a point whose Northing is 1369644.334 and whose Easting is 669323.451; thence bearing N 41-3-13.864 E a distance of 293.913; thence bearing N 5-5-2.201 W a distance of 123.838; thence bearing N 89-54-40.000 E a distance of 585.110; thence bearing S 0-30-16.244 E a distance of 897.286; thence bearing S 89-34-30.900 W a distance of 109.474; thence bearing N 27-10-11.222 W a distance of 344.529; thence bearing S 67-52-1.045 W a distance of 837.897; thence bearing S 75-46-40.726 W a distance of 504.832; thence bearing S 89-44-25.447 W a distance of 280.573; thence along a curve to the LEFT, having a radius of 282.593 a delta angle of 28° 24' 19.47", and whose long chord bears N 66-33-54.838 W a distance of 138.670; thence along a curve to the RIGHT, having a radius of 56.266 a delta angle of 63° 06' 44.52", and whose long chord bears N 50-2-14.386 W a distance of 58.892; thence bearing N 20-32-46.160 W a distance of 152.326; thence bearing N 69-41-52.000 E a distance of 76.000; thence bearing S 88-22-19.000 E a distance of 178.650; thence bearing N 88-22-46.662 E a distance of 190.685; thence bearing N 77-59-56.004 E a distance of 195.156; thence bearing N 70-22-13.996 E a distance of 277.616; thence bearing N 76-35-39.621 E a distance of 565.596; thence bearing N 48-22-54.083 W a distance of 239.952 to the point of beginning.

Tract 2

Beginning at a point whose Northing is 1367915.746 and whose Easting is 669607.933; thence bearing N 5-13-46.479 W a distance of 377.370; thence bearing N 54-13-46.919 E a distance of 61.645; thence bearing N 4-10-46.086 W a distance of 266.321; thence bearing S 55-48-6.657 W a distance of 263.144; thence bearing N 84-40-35.152 W a distance of 198.089; thence bearing S 74-13-52.034 W a distance of 339.880; thence bearing N 20-10-3.917 W a distance of 248.951; thence bearing N 63-9-29.350 E a distance of 909.256; thence bearing N 79-11-36.890 E a distance of 408.664; thence bearing N 89-34-30.900 E a distance of 109.474; thence bearing S 0-38-2.668 E a distance of 83.212; thence bearing S 0-24-45.929 E a distance of 117.500; thence bearing S 0-36-46.181 E a distance of 117.500; thence bearing S 0-24-9.103 E a distance of 124.796; thence bearing N 89-55-7.631 W a distance of 199.655; thence bearing S 0-1-42.588 E a distance of 660.088; thence bearing S 0-36-1.695 E a distance of 71.489; thence bearing S 89-25-42.236 W a distance of 286.048; thence bearing S 89-25-42.236 W a distance of 9.877 to the point of beginning.

Tract 3

Beginning at a point whose Northing is 1367915.845 and whose Easting is 669617.809; thence bearing N 89-25-42.236 E a distance of 286.048; thence bearing N 0-36-1.695 W a distance of 71.489; thence bearing S 89-44-47.620 E a distance of 200.438; thence bearing S 0-1-29.832 E a distance of 184.817; thence along a curve to the LEFT, having a radius of 590.000 a delta angle of 35° 26' 29.50", and whose long chord bears S 35-29-49.749 W a distance of 359.166; thence bearing S 17-46-35.000 W a distance of 99.611; thence bearing N 74-4-59.754 W a distance of 113.102; thence along a curve to the LEFT, having a radius of 475.000 a delta angle of 06° 33' 54.85", and whose long chord bears N 77-21-57.177 W a distance of 54.398; thence bearing N 80-27-18.850 W a distance of 260.447; thence bearing N 9-21-5.406 E a distance of 67.412; thence bearing N 72-42-43.516 E a distance of 54.746; thence bearing N 9-55-42.139 W a distance of 274.571; thence bearing N 71-39-54.955 E a distance of 165.020; thence bearing N 5-14-11.146 W a distance of 7.407 to the point of beginning.

Tract 4

Beginning at a point whose Northing is 1367448.229 and whose Easting is 669755.878; thence bearing S 74-4-59.754 E a distance of 113.102; thence bearing S 17-46-35.000 W a distance of 126.470; thence along a curve to the RIGHT, having a radius of 354.000 a delta angle of 24° 37' 20.10", and whose long chord bears S 30-5-15.051 W a distance of 150.960; thence bearing S 14-15-43.000 W a distance of 34.470; thence along a curve to the RIGHT, having a radius of 371.500 a delta angle of 06° 51' 06.27", and whose long chord bears S 50-31-1.867 W a distance of 44.400; thence bearing S 53-56-35.000 W a distance of 236.593; thence bearing N 55-31-55.505 W a distance of 172.173; thence bearing N 89-25-55.024 W a distance of 216.055; thence bearing S 59-29-30.316 W a distance of 55.004; thence along a curve to the RIGHT, having a radius of 606.263 a delta angle of 42° 28' 51.98", and whose long chord bears N 79-29-5.227 W a distance of 439.279; thence bearing N 27-45-46.000 E a distance of 118.260; thence along a curve to the RIGHT, having a radius of 472.500 a delta angle of 18° 01' 46.95", and whose long chord bears N 36-46-39.477 E a distance of 148.073; thence along a curve to the RIGHT, having a radius of 472.500 a delta angle of 16° 09' 51.75", and whose long chord bears N 53-52-28.830 E a distance of 132.861; thence bearing N 61-57-24.708 E a distance of 115.837; thence along a curve to the RIGHT, having a radius of 472.500 a delta angle of 37° 27' 05.43", and whose long chord bears N 80-40-57.421 E a distance of 303.382 ; thence bearing S 80-35-29.866 E a distance of 115.802; thence bearing S 80-27-18.850 E a distance of 260.447; thence along a curve to the RIGHT, having a radius of 475.000 a delta angle of 06° 33' 54.85", and whose long chord bears S 77-21-57.177 E a distance of 54.398 to the point of beginning.

Tract 5

Beginning at a point whose Northing is 1366745.048 and whose Easting is 668663.609; thence bearing N 74-57-40.102 W a distance of 30.000; thence bearing N 15-2-19.898 E a distance of 50.135; thence bearing N 74-23-16.329 W a distance of 491.651; thence bearing N 37-34-54.164 W a distance of 433.129; thence bearing S 52-27-7.000 W a distance of 120.000; thence bearing N 37-32-53.000 W a distance of 63.420; thence bearing N 52-27-7.000 E a distance of 120.000; thence bearing N 37-32-53.000 W a distance of 262.500; thence bearing N 52-27-43.156 E a distance of 208.463; thence along a curve to the LEFT, having a radius of 1000.769a delta angle of 33° 46' 42.47", and whose long chord bears S 45-21-42.530 E a distance of 581.492; thence bearing S 62-11-29.851 E a distance of 453.554; thence along a curve to the LEFT, having a radius of 666.103a delta angle of 15° 40' 37.68", and whose long chord bears S 70-16-5.261 E a distance of 181.690; thence bearing S 11-12-15.283 W a distance of 46.226; thence bearing S 44-57-8.085 W a distance of 268.948to the point of beginning.

Tract 6

Beginning at a point whose Northing is 1366233.362 and whose Easting is 668008.192; thence bearing N 69-53-8.899 E a distance of 272.187; thence bearing N 49-1-18.527 E a distance of 443.075; thence bearing N 15-2-19.898 E a distance of 140.110; thence bearing S 74-57-40.102 E a distance of 30.000; thence bearing N 44-57-8.085 E a distance of 268.948; thence bearing N 11-12-15.283 E a distance of 46.226; thence along a curve to the LEFT, having a radius of 666.103a delta angle of 11° 18' 04.11", and whose long chord bears S 83-45-26.153 E a distance of 131.171; thence bearing S 0-54-1.000 E a distance of 11.058; thence along a curve to the LEFT, having a radius of 680.200a delta angle of 04° 25' 32.30", and whose long chord bears N 88-23-50.152 E a distance of 52.527; thence bearing N 86-11-4.000 E a distance of 55.403; thence along a curve to the RIGHT, having a radius of 369.670a delta angle of 57° 26' 11.01", and whose long chord bears S 65-5-50.493 E a distance of 355.254; thence bearing S 32-31-36.943 E a distance of 50.012; thence bearing S 60-44-55.000 W a distance of 422.389; thence along a curve to the LEFT, having a radius of 5879.580a delta angle of 08° 44' 58.32", and whose long chord bears S 56-22-25.838 W a distance of 896.988; thence bearing N 89-38-40.621 W a distance of 93.162; thence bearing S 51-14-19.171 W a distance

of 119.719; thence bearing N 89-50-54.000 W a distance of 212.951; thence bearing N 12-52-54.534 W a distance of 61.227; thence bearing N 77-7-5.466 E a distance of 125.000; thence bearing N 12-52-54.534 W a distance of 157.914 to the point of beginning.

Tract 7

Beginning at a point whose Northing is 1366822.680 and whose Easting is 667873.417; thence bearing S 12-52-54.534 E a distance of 604.533; thence bearing S 12-52-54.534 E a distance of 157.914; thence bearing S 77-7-5.466 W a distance of 125.000; thence bearing S 12-52-54.534 E a distance of 61.227; thence bearing N 89-50-54.000 W a distance of 143.519; thence bearing N 0-13-57.000 E a distance of 190.000; thence bearing N 89-46-3.000 W a distance of 110.000; thence bearing N 0-13-57.000 E a distance of 10.000; thence bearing S 89-46-3.000 E a distance of 90.000; thence bearing N 0-13-57.000 E a distance of 60.000; thence bearing N 89-46-3.000 W a distance of 20.000; thence bearing N 0-13-57.000 E a distance of 180.000; thence bearing N 89-46-3.000 W a distance of 44.230; thence bearing N 0-13-57.000 E a distance of 129.980; thence bearing N 89-45-18.311 W a distance of 90.770; thence bearing S 0-13-57.000 W a distance of 50.000; thence bearing N 89-49-55.966 W a distance of 120.007; thence bearing S 0-13-57.000 W a distance of 110.000; thence bearing N 89-46-3.000 W a distance of 85.000; thence bearing S 0-13-57.000 W a distance of 140.000; thence bearing N 74-30-44.000 W a distance of 114.020; thence bearing N 89-46-3.000 W a distance of 190.000; thence bearing S 76-11-46.000 W a distance of 123.690; thence bearing N 89-46-3.000 W a distance of 250.000; thence bearing N 0-13-57.000 E a distance of 62.330; thence along a curve to the RIGHT, having a radius of 138.830 a delta angle of 61° 56' 15.00", and whose long chord bears N 31-12-4.500 E a distance of 142.876; thence bearing N 62-10-11.985 E a distance of 0.003; thence bearing N 62-10-12.000 E a distance of 165.990; thence bearing N 56-26-18.000 E a distance of 360.000; thence bearing N 37-11-42.000 E a distance of 401.480; thence bearing N 51-14-17.000 E a distance of 289.840; thence bearing S 37-18-51.035 E a distance of 169.900; thence bearing S 52-25-1.745 W a distance of 119.087; thence bearing S 37-32-30.858 E a distance of 179.989; thence bearing S 22-7-58.159 E a distance of 59.970 to the point of beginning.

Tract 8

Beginning at a point whose Northing is 1367436.181 and whose Easting is 666551.405; thence bearing S 7-36-17.000 W a distance of 1277.033; thence bearing N 89-46-3.000 W a distance of 165.003; thence bearing N 5-8-48.000 W a distance of 800.000; thence bearing N 11-35-59.000 E a distance of 327.190; thence bearing S 89-46-3.000 E a distance of 12.410; thence bearing N 10-52-60.000 E a distance of 192.440; thence bearing N 45-12-39.000 E a distance of 167.370; thence along a curve to the LEFT, having a radius of 200.000 a delta angle of 40° 24' 38.57", and whose long chord bears S 38-51-27.716 E a distance of 138.154; thence bearing S 59-3-47.000 E a distance of 100.000 to the point of beginning.

Tract 9A

Beginning at a point whose Northing is 1368477.359 and whose Easting is 667502.505; thence along a curve to the RIGHT, having a radius of 796.194 a delta angle of 01° 49' 36.51", and whose long chord bears S 25-53-50.430 E a distance of 25.385; thence along a curve to the RIGHT, having a radius of 35.000 a delta angle of 85° 08' 38.24", and whose long chord bears S 18-13-42.608 W a distance of 47.356; thence bearing S 60-48-1.727 W a distance of 156.126; thence along a curve to the LEFT, having a radius of 350.000 a delta angle of 44° 20' 39.18", and whose long chord bears S 38-37-42.135 W a distance of 264.173; thence along a curve to the RIGHT, having a radius of 309.351 a delta angle of 31° 50' 43.77", and whose long chord bears N 7-56-24.687 W a distance of 169.736; thence bearing N 7-50-5.743 E a distance of 17.977; thence bearing N 12-51-43.890 E a distance of 28.456; thence bearing N 17-53-22.037 E a distance of 28.456; thence bearing N 22-55-0.184 E a distance of 28.456; thence bearing N 27-56-38.331 E a distance of 28.456; thence bearing N 32-58-16.478 E a distance of 28.456; thence bearing N 37-59-54.624 E a distance of 28.456; thence

bearing N 43-1-32.771 E a distance of 28.456; thence bearing N 48-3-10.918 E a distance of 28.456; thence bearing N 53-4-49.065 E a distance of 28.456; thence bearing N 58-6-27.212 E a distance of 28.456; thence bearing N 63-8-5.359 E a distance of 28.456; thence bearing N 65-42-35.946 E a distance of 35.830; thence bearing N 61-30-25.266 E a distance of 5.303; thence bearing N 53-4-16.284 E a distance of 5.303; thence bearing N 44-38-7.301 E a distance of 5.303; thence bearing N 36-11-58.319 E a distance of 5.303; thence bearing N 27-45-49.337 E a distance of 5.433; thence bearing S 43-37-28.028 E a distance of 81.381; thence along a curve to the RIGHT, having a radius of 200.000 a delta angle of 16° 38' 37.32", and whose long chord bears S 35-18-9.369 E a distance of 57.893 to the point of beginning.

Tract 9B

Beginning at a point whose Northing is 1368278.290 and whose Easting is 667497.625; thence bearing S 0-14-21.069 W a distance of 522.623; thence bearing S 63-20-6.000 W a distance of 532.072; thence bearing S 52-41-59.959 W a distance of 253.176; thence bearing N 59-7-48.924 W a distance of 116.481; thence bearing N 30-56-13.000 E a distance of 25.000; thence bearing N 59-3-47.000 W a distance of 70.000; thence bearing N 45-22-54.685 E a distance of 560.063; thence bearing S 28-21-16.794 E a distance of 7.064; thence along a curve to the LEFT, having a radius of 360.000a delta angle of 47° 41' 16.66", and whose long chord bears N 37-48-4.876 E a distance of 291.058; thence along a curve to the RIGHT, having a radius of 290.000a delta angle of 46° 50' 35.18", and whose long chord bears N 37-22-44.136 E a distance of 230.546; thence bearing N 60-48-1.727 E a distance of 145.630; thence along a curve to the RIGHT, having a radius of 35.000a delta angle of 94° 33' 46.24", and whose long chord bears S 71-55-5.153 E a distance of 51.429; thence bearing S 24-38-12.033 E a distance of 64.017; thence bearing N 89-46-0.785 W a distance of 96.839 to the point of beginning.

Tract 10

A legal description for the revised Tract 10 will be provided prior to the City Commission hearing.

Tract 11

A legal description for the revised Tract 11 will be provided prior to the City Commission hearing.

Tract 12 (Formerly part of Area U)

Being a portion of the North ½ of Section 36, Township 27 South, Range 23 East, Polk County, Florida, more particularly described as follows:

Commence at the southwest corner of "Fairfield on the Tee", according of the plat thereof, as recorded in Plat Book 78, Page 24, of the public records of Polk County, Florida, said point being on the arc of a circular curve concave to the northeast and bearing S63°28'10"W from the center of said curve, said point also being on the northerly right-of-way line of Carpenters Way as recorded in official records book 4055, page 644, of the public records of Polk County, Florida; thence southeasterly along the arc of said curve with a radius of 942.44 feet, through a central angle of 13°14'46", a distance of 217.88 feet; thence continue southeasterly, along said northerly right-of-way line and along said curve through a central angle of 22°27'36", a distance of 369.45 feet; thence, continue along said northerly right-of-way line S62°08'12"E (S62°14'14"E per historic deed), a distance of 91.13 feet to the point of beginning; thence N27°25'51"E, a distance of 100.76 feet; thence N64°27'26"E, a distance of 244.38 feet; thence S56°01'07"E (S56°00'03"E per historic deed), a distance of 170.71 feet; thence S32°43'25"E (S32°50'30" per historic deed), a distance of 48.04 feet (48.13' per historic deed); thence S55°22'26"E (S55°24'36"E per historic deed), a distance of 161.05 feet (161.03' per historic deed); thence S75°20'55"W (S75°20'02"W per historic deed), a distance of 154.15 feet (154.24' per historic deed); thence S31°57'55"W (S31°57'46"W per historic deed), a distance of 45.20 feet (45.17' per historic deed); thence S13°34'12"W (S13°26'54"W per historic deed), a distance of 57.17 feet (57.26'

per historic deed); thence $S08^{\circ}49'51"E$ ($S09^{\circ}39'34"E$ per historic deed), a distance of 33.77 feet (33.20' per historic deed) to the point of curvature of a non tangent curve to the right, having a radius of 599.21 feet, a central angle of $06^{\circ}51'38"$ ($06^{\circ}52'14"$ per historic deed), a chord bearing of $N65^{\circ}42'41"W$, for a chord distance of 71.71 feet; thence along the arc of said curve, a distance of 71.75 feet (71.85' per historic deed); thence $N62^{\circ}08'12"W$ ($N62^{\circ}14'14"W$ per historic deed), a distance of 362.30 feet to the point of beginning, and containing 2.841 acres (108.073 square feet) of land, more or less.

WEDGEWOOD MASTER PLAN
 DECEMBER 6, 2022

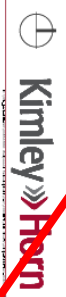
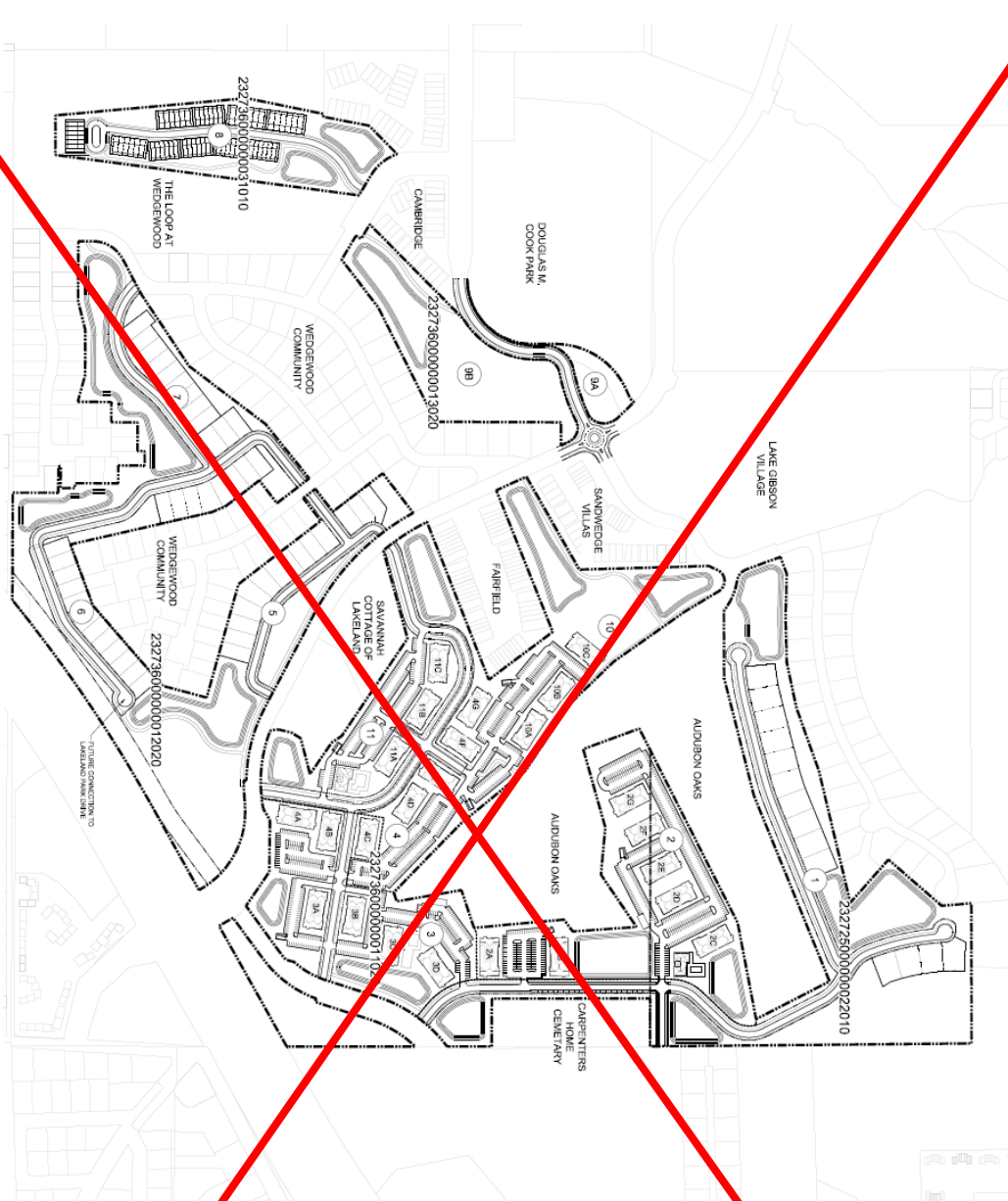


FIGURE SUMMARY SITE DATA TABLE

DEVELOPMENT AREA	TOTAL AREA (AC)	TOTAL LOTS	DENSITY (LTS/AC)	DEVELOPMENT TYPE
DEVELOPMENT AREA 1	103.84 AC	103	1.00	RESIDENTIAL
DEVELOPMENT AREA 2	103.84 AC	103	1.00	RESIDENTIAL
DEVELOPMENT AREA 3	103.84 AC	103	1.00	RESIDENTIAL
DEVELOPMENT AREA 4	103.84 AC	103	1.00	RESIDENTIAL
DEVELOPMENT AREA 5	103.84 AC	103	1.00	RESIDENTIAL
DEVELOPMENT AREA 6	103.84 AC	103	1.00	RESIDENTIAL
DEVELOPMENT AREA 7	103.84 AC	103	1.00	RESIDENTIAL
DEVELOPMENT AREA 8	103.84 AC	103	1.00	RESIDENTIAL
DEVELOPMENT AREA 9	103.84 AC	103	1.00	RESIDENTIAL
DEVELOPMENT AREA 10	103.84 AC	103	1.00	RESIDENTIAL
DEVELOPMENT AREA 11	103.84 AC	103	1.00	RESIDENTIAL
DEVELOPMENT AREA 12	103.84 AC	103	1.00	RESIDENTIAL
DEVELOPMENT AREA 13	103.84 AC	103	1.00	RESIDENTIAL
DEVELOPMENT AREA 14	103.84 AC	103	1.00	RESIDENTIAL
DEVELOPMENT AREA 15	103.84 AC	103	1.00	RESIDENTIAL
DEVELOPMENT AREA 16	103.84 AC	103	1.00	RESIDENTIAL
TOTAL DEVELOPMENT AREA	1,661.44 AC	1,661	1.00	RESIDENTIAL

Exhibit "W-3"

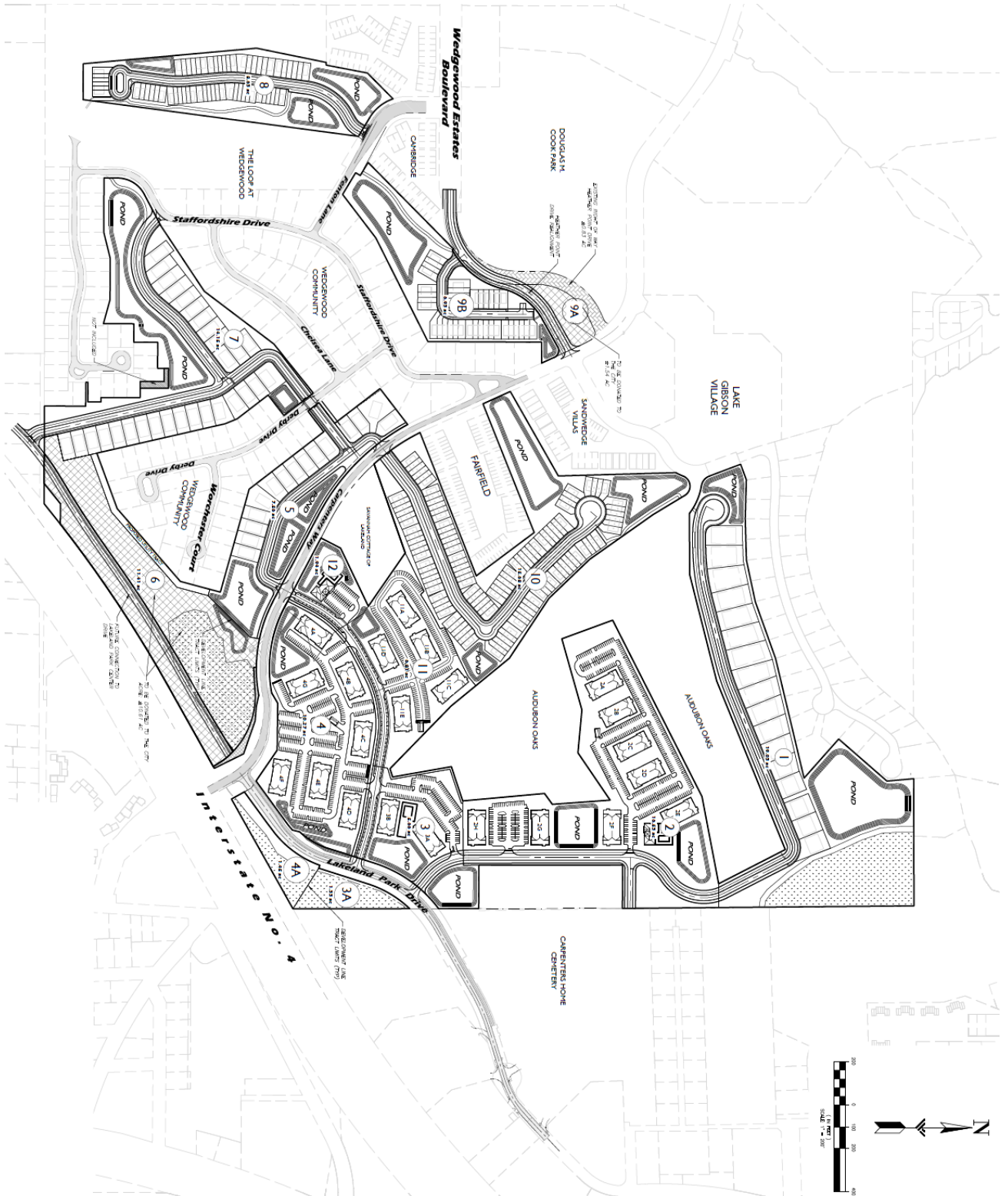
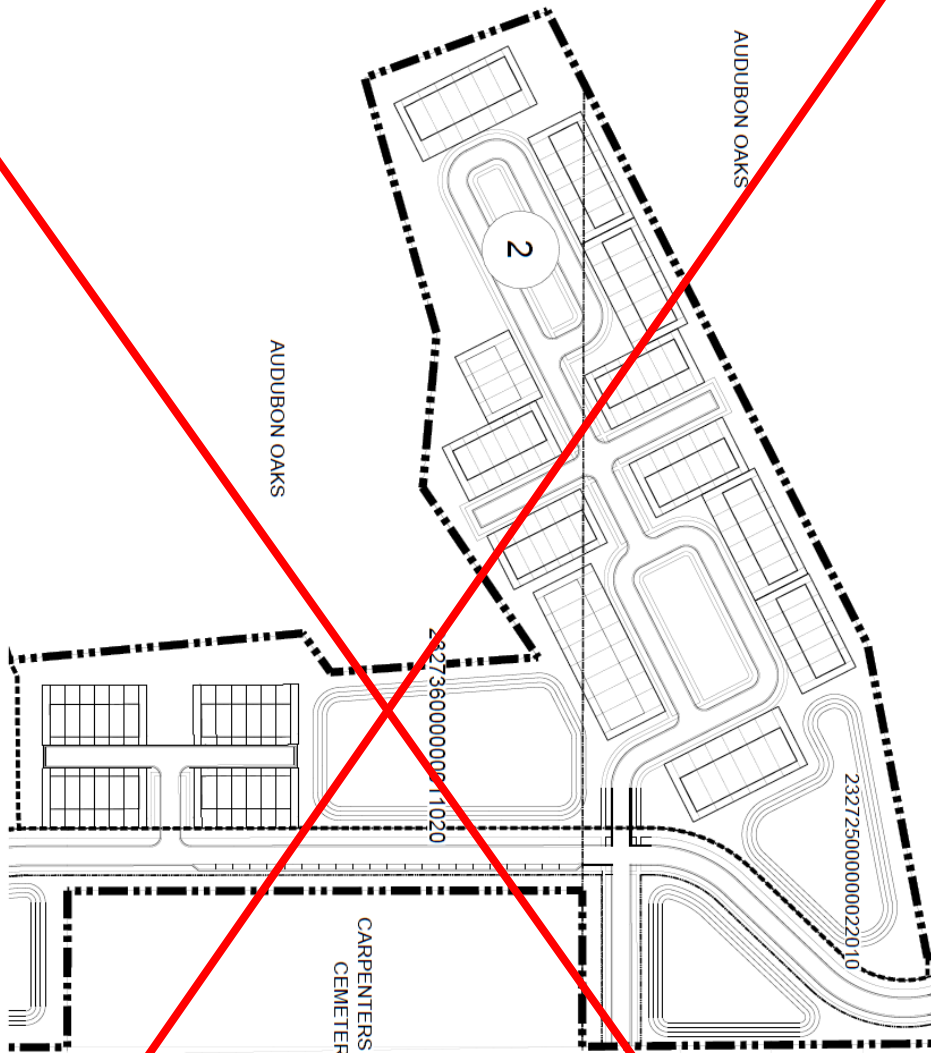


Exhibit "W-3a"



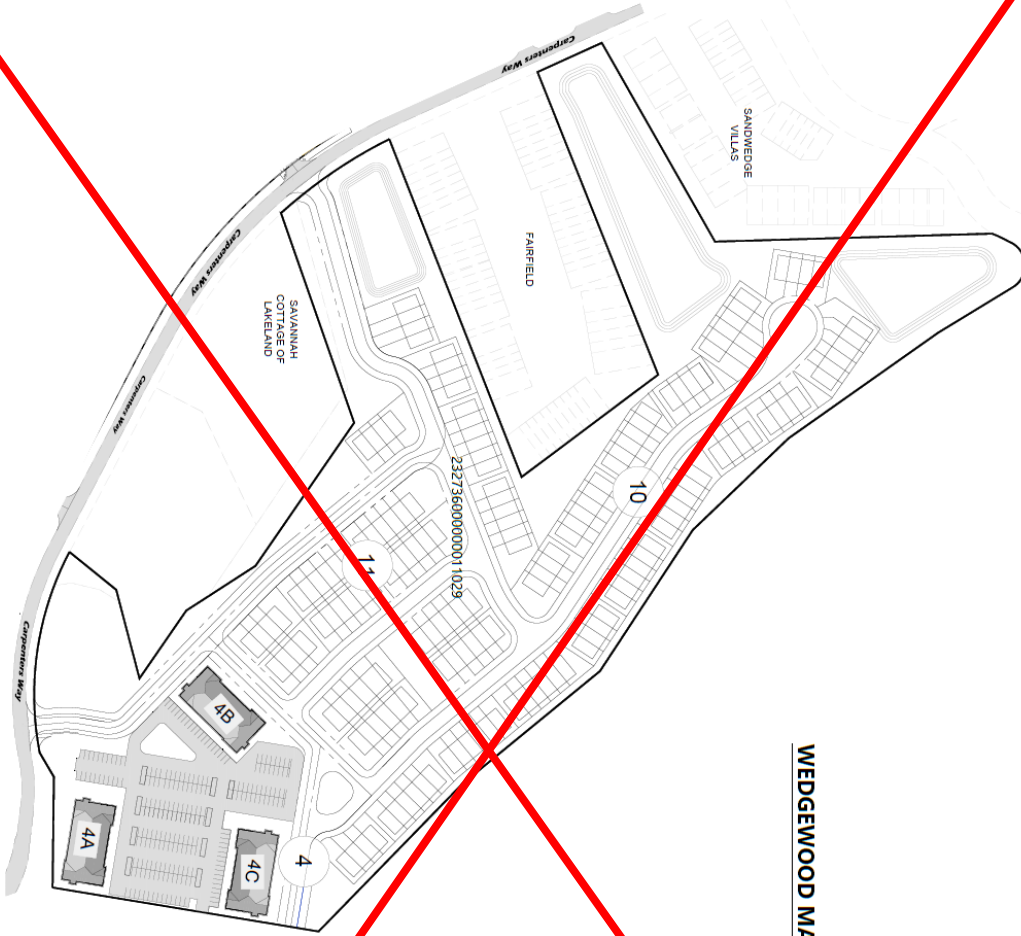
TOWNHOME PARCELS

DEVELOPMENT AREA 2	EXISTING FFLU	PROPOSED FFLU	PHASE
DEVELOPMENT AREA 2	RM	RM1	III
TOTAL ACRES			
TOTAL UNITS			
DENSITY (22' X 75')			
DENSITY (19' X 77')			
DENSITY (5' X 50')			

WEDGWOOD MASTER PLAN - DEVELOPMENT AREA 2

NOVEMBER 8, 2023





WEDGEWOOD MASTER PLAN - DEVELOPMENT AREA 4, 10 & 11

PRELIMINARY SITE DATA TABLE

MULTI-FAMILY PARCELS

DEVELOPMENT AREA 4:
 UNITS ± 120
 PARKING REQUIRED ± 216 (1.8 RATIO)
 1 BEDROOM / STUDIO ± 81 (1.5 PER UNIT)
 2+ BEDROOM ± 72 (2 PER UNIT)

BUILDING DATA	PHASE
4B 4-STORY / 40 UNITS	II
4C 4-STORY / 40 UNITS	II

TOWNHOMES PARCELS

DEVELOPMENT AREA 10 & 11:
 UNITS ± 180 UNITS
 (22 X 87)
 (19 X 77)
 TOTAL UNITS ± 180 UNITS

EXISTING BLU	PHASE
NM	NM

DEVELOPMENT AREA 4, 10 & 11:
 TOTAL UNITS ± 300 UNITS
 DENSITY ± 9.0 DU/AC

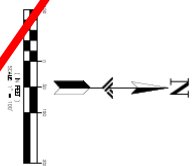
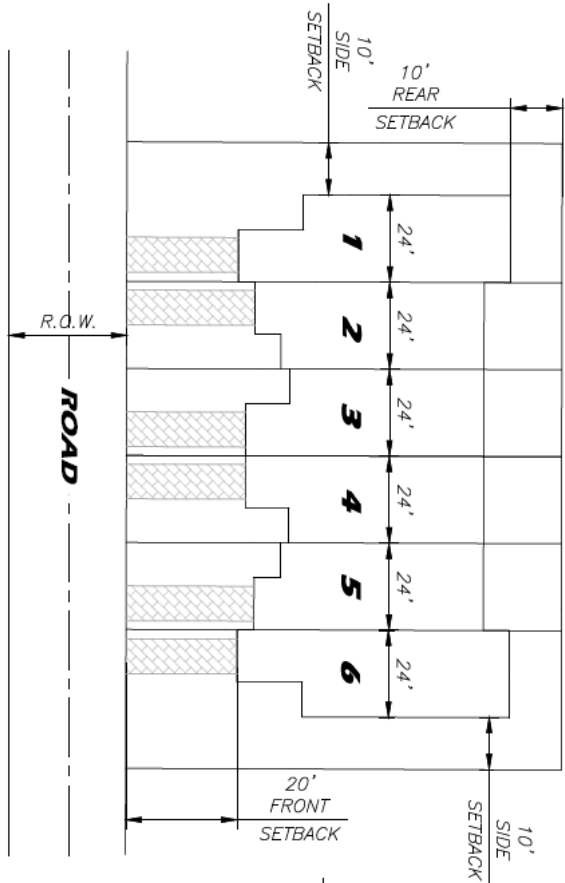


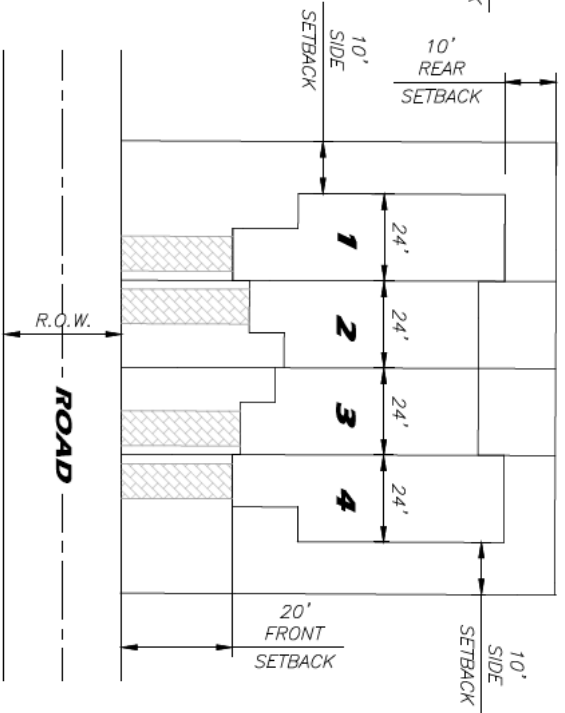
Exhibit "W-3a"

LOT LAYOUT TOWNHOUSE



**LOT LAYOUT
6 UNITS TOWNHOUSE**
N.T.S.

NOTE: GARAGES SHALL BE RECESSED TO 25 FEET



**LOT LAYOUT
4 UNITS TOWNHOUSE**
N.T.S.

NOTE: GARAGES SHALL BE RECESSED TO 25 FEET

Exhibit "W 4a"

LANDSCAPE BUFFERS (EXHIBIT W-4A)

DECEMBER 7, 2022

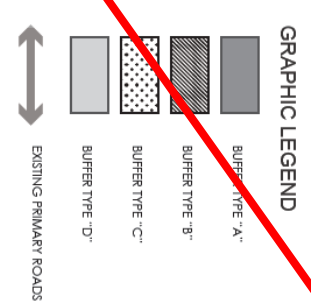


Exhibit "W-4a"

3

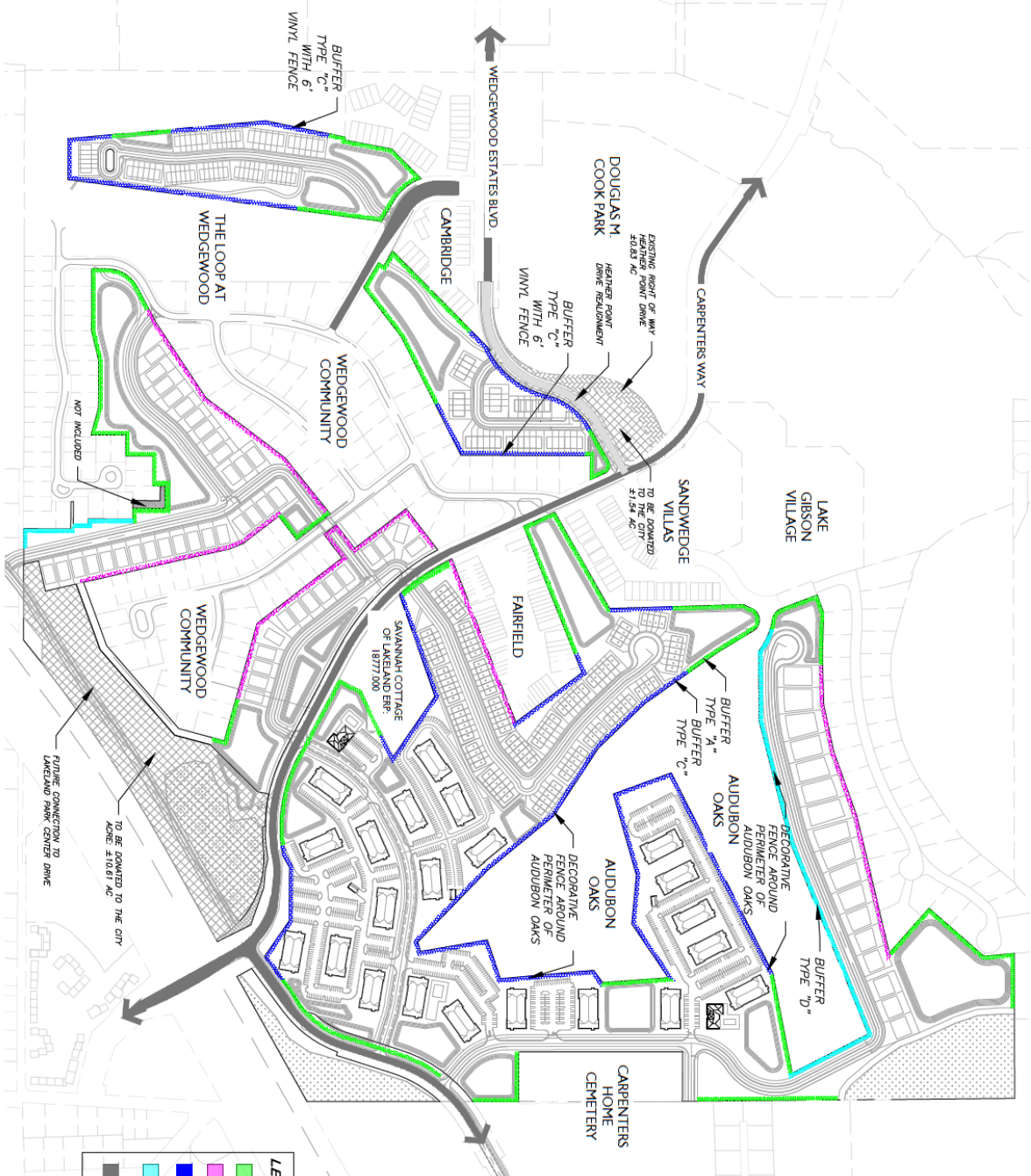
LANDSCAPE BUFFERS

ATTACHMENT "W-4a"

SCALE: 1" = 350'



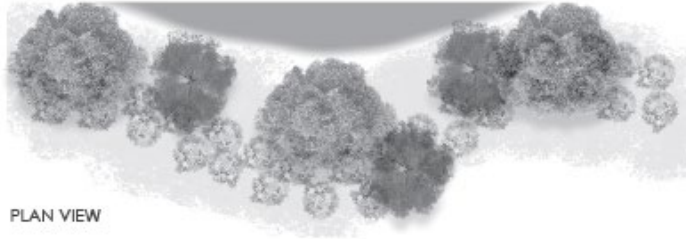
TRACT ENGINEERING
FROM CONCEPTUAL TO COMPLETION
3010 SANDHURST DRIVE
SAN ANTONIO, TEXAS 78201
PH: 214.520.1100 FAX: 214.520.1101



LEGEND

█	BUFFER TYPE "A"
█	BUFFER TYPE "B"
█	BUFFER TYPE "C"
█	BUFFER TYPE "D"
█	EXISTING PRIMARY ROADS

Exhibit "W-4b"



PLAN VIEW

A TYPE 'A' LANDSCAPE BUFFER

TYPICAL PER EVERY 100 LF

- 3 canopy trees, 3 understory trees, 30 shrubs, 2'-3' height berm
- shrubs to be placed in natural, organic patterns

PLAN VIEW



B TYPE 'B' LANDSCAPE BUFFER

TYPICAL PER EVERY 100 LF

- 4 canopy trees, 6 understory trees, 50 shrubs, 2'-3' height berm
- shrubs to be placed in natural / organic patterns

PLAN VIEW

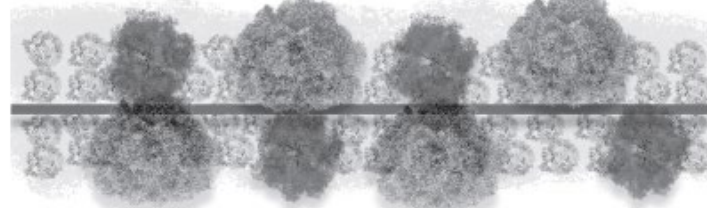


C TYPE 'C' LANDSCAPE BUFFER

TYPICAL PER EVERY 100 LF

- 2 canopy trees, 2 understory trees, 30 shrubs
- shrubs to be placed in natural / organic patterns

PLAN VIEW

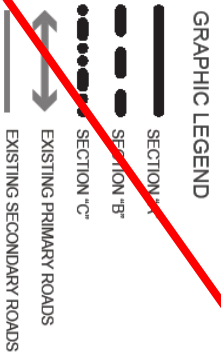


D TYPE 'D' LANDSCAPE BUFFER

TYPICAL PER EVERY 100 LF

- 4 trees, 4 understory trees, 30 shrubs (each side of wall), optional 6' ht. screen wall

LANDSCAPE BUFFER - SECTIONS



SITE CIRCULATION - ROADWAY (EXHIBIT W-5A)

DECEMBER 6, 2022



Kimley-Horn

Exhibit "W-5a"

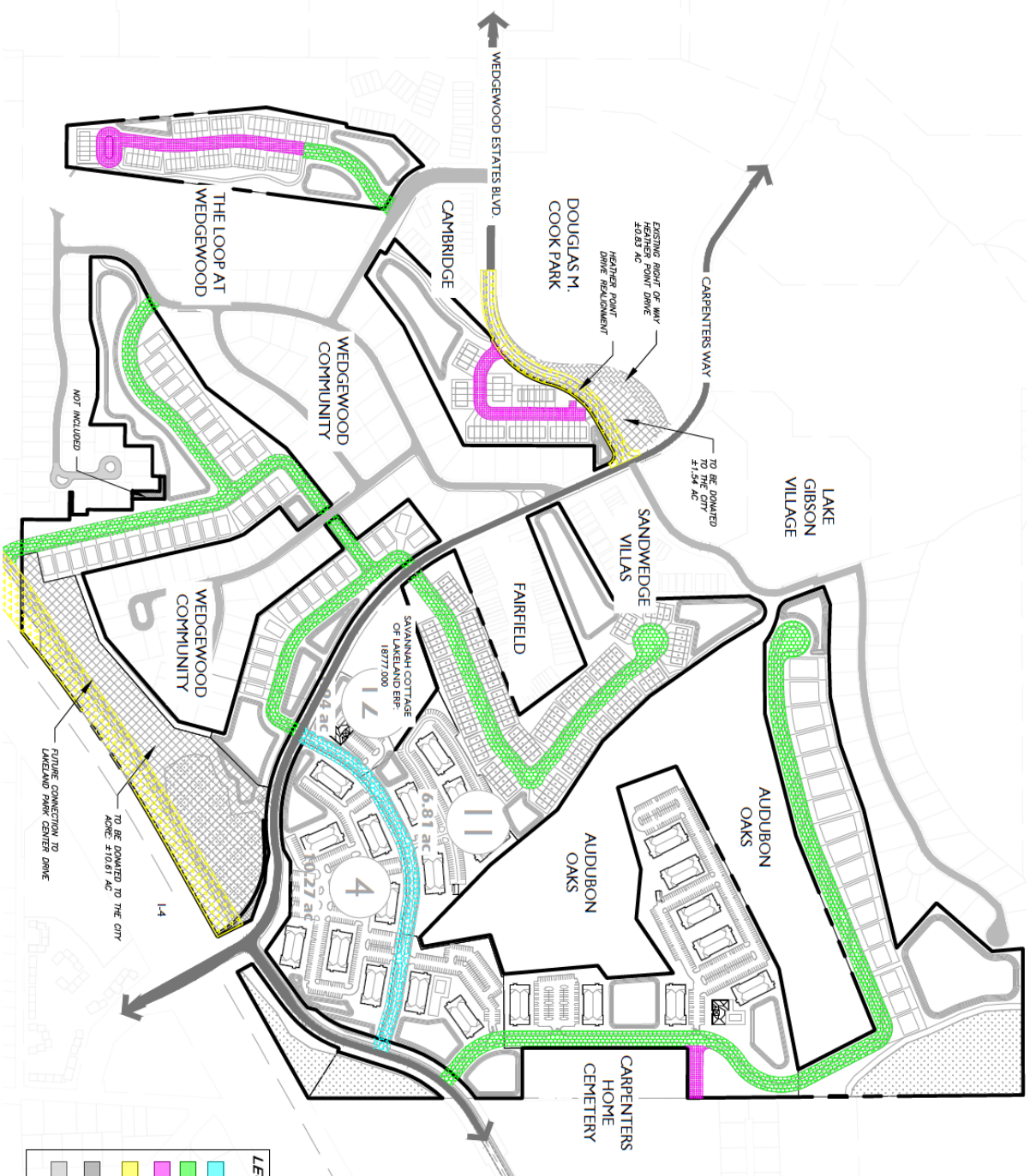
1

SITE CIRCULATION - ROW

ATTACHMENT "W-5a"



TRACT ENGINEERING
FROM CONCEPTUAL TO COMPLETION
3177 LAKELAND BLVD, SUITE 100
IRVING, TEXAS 75039



LEGEND	
	SECTION "A"
	SECTION "B"
	SECTION "C"
	CITY
	EXISTING PRIMARY ROADS
	EXISTING SECONDARY ROADS

DECEMBER 6, 2022
SITE CIRCULATION - PEDESTRIAN (EXHIBIT W-5B)



GRAPHIC LEGEND

- SECTION 'D'
- ↔ EXISTING PRIMARY ROADS



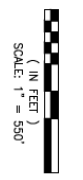
Kimley»Horn

Exhibit "W-5b"

2

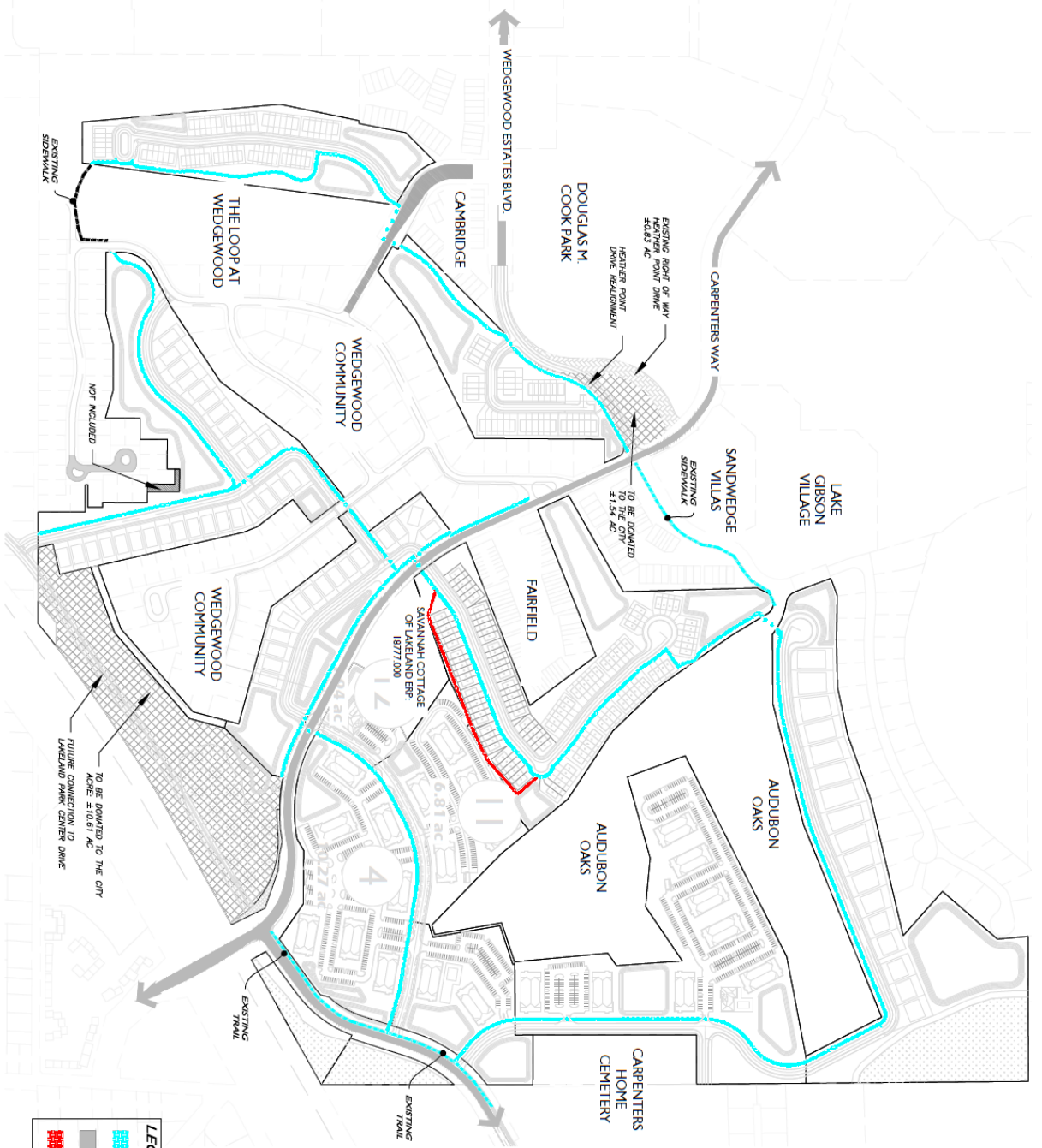
SITE CIRCULATION - PEDESTRIAN

ATTACHMENT "W-5b"



TRACT ENGINEERING
FROM CONCEPTUAL TO COMPLETION

5117 LAKELAND BL, SUITE 100
ANN ARBOR, MI 48106-1443
PHONE: 734.769.8800



LEGEND

	SECTION "B"
	EXISTING PRIMARY ROADS
	SECTION "D1"

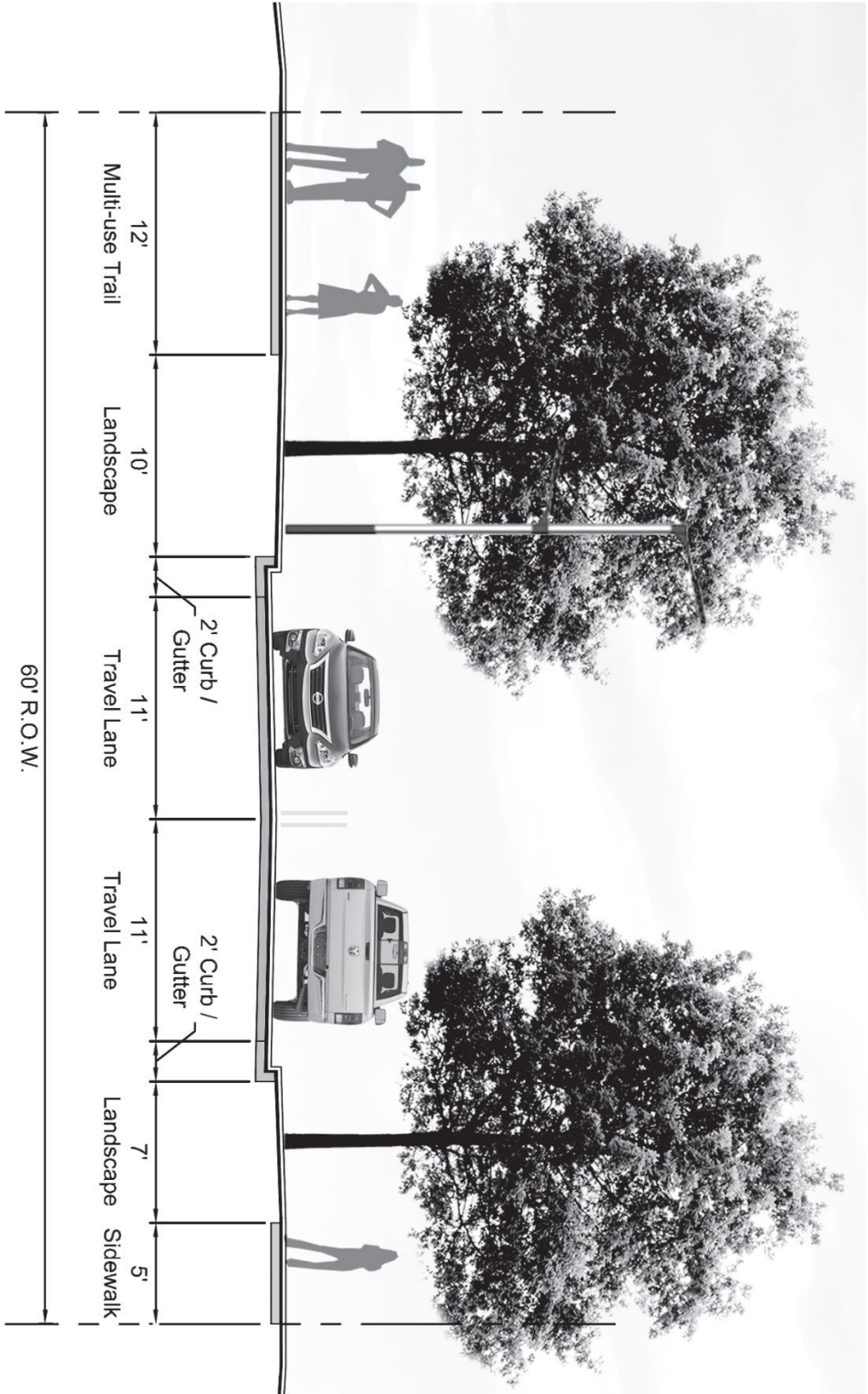
Exhibit "W-6a"



A

SECTION 'A'

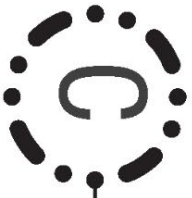
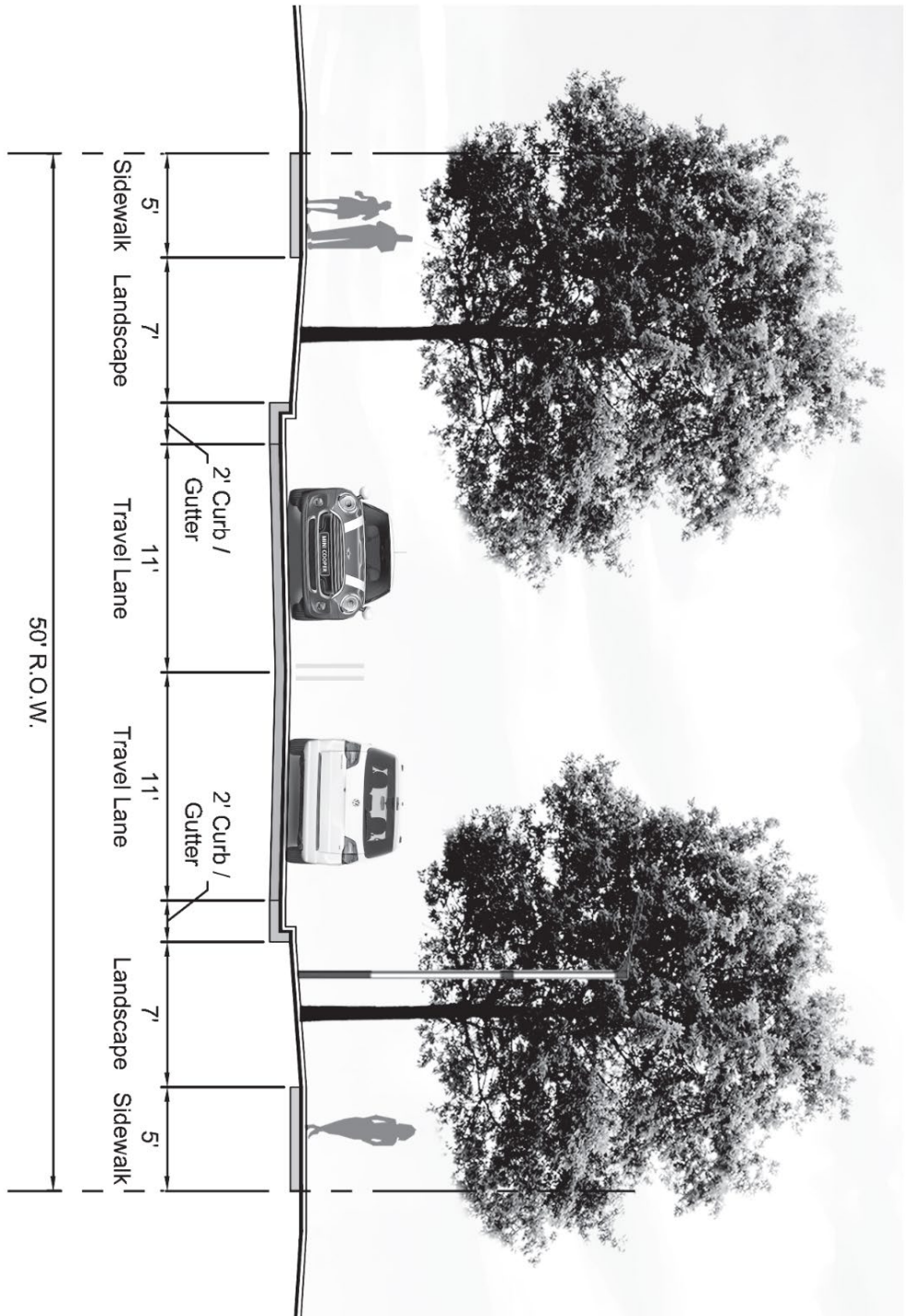
(64' R.O.W. with 2-way traffic, on-street parking, multi-use trail, and landscaping)



SECTION 'B'

(60' R.O.W. with 2-way traffic, multi-use trail, and landscaping)

Exhibit "W-6c"



SECTION 'C'

(50' R.O.W. with 2-way traffic, standard sidewalk, and landscaping)

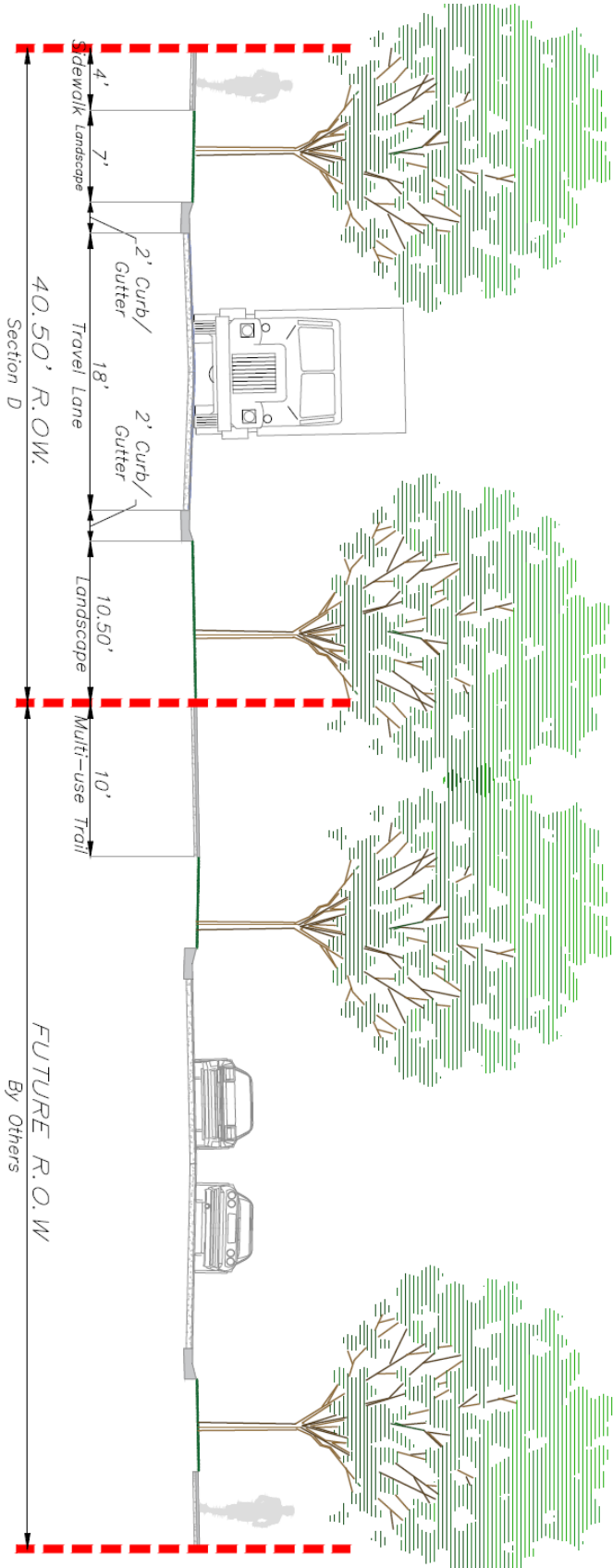


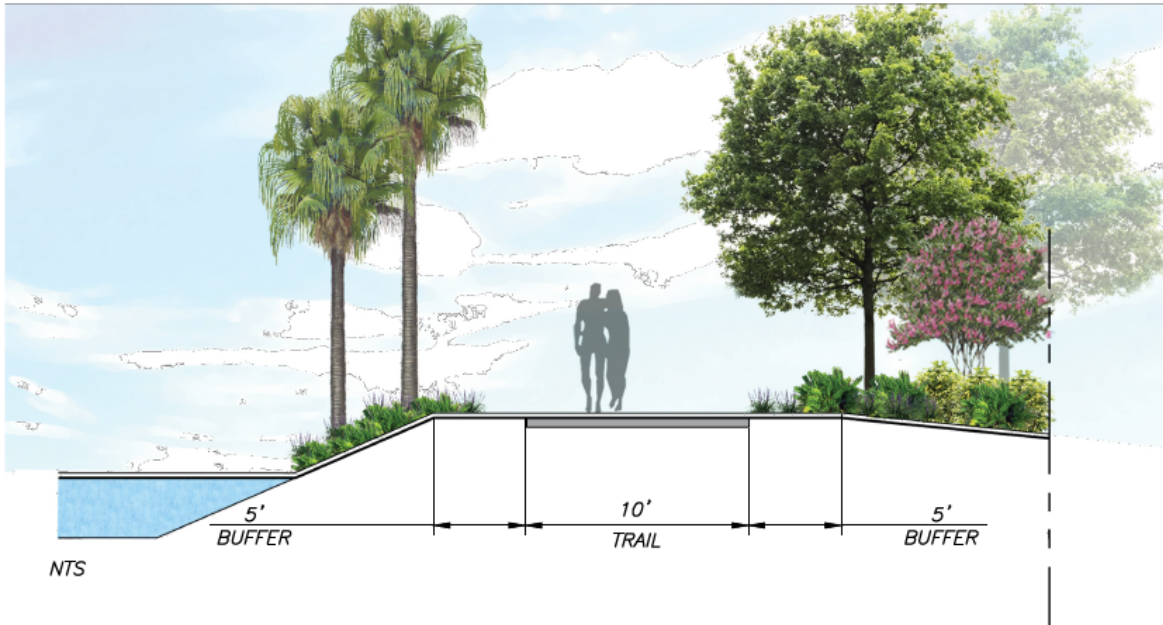
SECTION 'D'

(multi-use trail / greenway open space)

Exhibit "W-6e"

SECTION "E" — ROW





SECTION "D1"

**WEDGE
WOOD**

TRACT ENGINEERING
FROM CONCEPTUAL TO COMPLETION



5137 S LAKELAND DR, SUITE 3
LAKELAND, FL 33813
FIRM REGISTRATION NUMBER - 34343

Exhibit "W-7"

PRELIMINARY SITE DATA TABLE

SINGLE FAMILY PARCELS

		EXISTING FLU	PROPOSED FLU	PHASE
DEVELOPMENT AREA 1:				
TOTAL AREA	± 19.04 AC	RM	RL	II
WETLANDS	± 3.33 AC			
DEVELOPABLE AREA:	± 15.71 AC			
UNITS (90' WIDE):	± 14			
DENSITY:	± 0.89 DU/AC			
DEVELOPMENT AREA 5:				
TOTAL AREA	± 13.02 AC	RL	RL	I
WETLANDS	± 2.99 AC			
ROW DEDICATION	± 1.31 AC			
DEVELOPABLE AREA:	± 8.72 AC			
UNITS (60' WIDE):	± 14			
DENSITY:	± 1.61 DU/AC			
DEVELOPMENT AREA 6:				
TOTAL AREA	± 9.18 AC	RL	RL	I
ROW DEDICATION	± 1.42 AC			
DEVELOPABLE AREA:	± 7.76 AC			
UNITS (60' WIDE):	± 12			
DENSITY:	± 1.55 DU/AC			
DEVELOPMENT AREA 7:				
DEVELOPABLE AREA:	± 11.21 AC	RL	RL	I
UNITS (60' WIDE):	± 20			
DENSITY:	± 1.78 DU/AC			
TOTAL SINGLE FAMILY UNITS:	± 60 UNITS			

TOWNHOME PARCELS

		EXISTING FLU	PROPOSED FLU	PHASE
DEVELOPMENT AREA 8:				
DEVELOPABLE AREA	± 8.63 AC	RH	RH	II
TOTAL UNITS:	± 60 UNITS			
(24' X 83')	± 12 UNITS			
(22' X 76')	± 19 UNITS			
(19' X 77')	± 36 UNITS			
DENSITY:	± 6.95 DU/AC			
TOTAL TOWNHOME UNITS:	± 60 UNITS			

MULTIFAMILY PARCELS

DEVELOPMENT AREA 2:				
DEVELOPABLE AREA:	± 16.55 AC	BUILDING DATA:		PHASE
UNITS:	± 230 ←	2A	4-STORY / 40 UNITS	III
DENSITY:	± 13.90 DU/AC	2B	4-STORY / 40 UNITS	III
PARKING PROVIDED:	± 295 (2.2 RATIO)	2C	3-STORY / 30 UNITS	III
PARKING REQUIRED:	± 414	2D	3-STORY / 30 UNITS	III
1 BEDROOM/STUDIO	± 92 (1.5 PER UNIT)	2E	3-STORY / 30 UNITS	III
2+ BEDROOM	± 138 (2 PER UNIT)	2F	3-STORY / 30 UNITS	III
		2G	3-STORY / 30 UNITS	III
DEVELOPMENT AREA 3:				
TOTAL AREA	± 12.46 AC	BUILDING DATA:		PHASE
WETLANDS	± 2.77 AC	3A	4-STORY / 40 UNITS	II
DEVELOPABLE AREA:	± 9.69 AC	3B	4-STORY / 40 UNITS	II
UNITS:	± 154 ←	3C	4-STORY / 34 UNITS	II
DENSITY:	± 15.89 DU/AC	3D	4-STORY / 40 UNITS	II
PARKING PROVIDED:	± 264 (1.7 RATIO)			
PARKING REQUIRED:	± 277			
1 BEDROOM/STUDIO	± 62 (1.5 PER UNIT)			
2+ BEDROOM	± 92 (2 PER UNIT)			
DEVELOPMENT AREA 4/10/11:				
DEVELOPABLE AREA:	± 28.31 AC	BUILDING DATA:		PHASE
UNITS:	± 450 ←	4A	4-STORY / 40 UNITS	II
DENSITY:	± 15.90 DU/AC	4B	4-STORY / 40 UNITS	II
PARKING PROVIDED:	± 861 (1.9 RATIO)	4C	4-STORY / 40 UNITS	II
PARKING REQUIRED:	± 810	4D	4-STORY / 40 UNITS	I
1 BEDROOM/STUDIO	± 180 (1.5 PER UNIT)	4E	4-STORY / 40 UNITS	I
2+ BEDROOM	± 270 (2 PER UNIT)	4F	4-STORY / 40 UNITS	I
		4G	4-STORY / 40 UNITS	I
		10A	2-STORY / 20 UNITS	I
		10B	2-STORY / 20 UNITS	I
		10C	2-STORY / 20 UNITS	I
		11A	4-STORY / 40 UNITS	I
		11B	4-STORY / 40 UNITS	I
		11C	3-STORY / 30 UNITS	I
TOTAL MULTIFAMILY UNITS:	± 834 UNITS			
BUILDINGS (80' X 157')	± 24			
PHASE 1	± 330 UNITS			
PHASE 2	± 274 UNITS			
PHASE 3	± 230 UNITS			

Exhibit "W-7"

PRELIMINARY SITE DATA TABLE					MULTI-FAMILY			
					EXISTING FLU	PROPOSED FLA	PHASE	
DETACHED SINGLE FAMILY (MAX. 2 STORY)								
	EXISTING FLU	PROPOSED FLA	PHASE					
DEVELOPMENT AREA 1					RL	RL	II	
TOTAL AREA	19.03 AC							
WETLANDS	3.79 AC							
DEVELOPABLE AREA	15.24 AC							
UNITS (60' WIDE)	17 DU							
DENSITY	0.89 DU/AC							
DEVELOPMENT AREA 5					RL	RL	II	
TOTAL AREA	7.56							
WETLANDS	0							
DEVELOPABLE AREA	7.56							
UNITS (60' WIDE)	15							
DENSITY	1.98							
DEVELOPMENT AREA 6					RL	RL	II	
TOTAL AREA	11.41 AC							
WETLANDS	3.25 AC							
DEVELOPABLE AREA	8.16 AC							
UNITS (60' WIDE)	0 DU							
DENSITY	0.00 DU/AC							
DEVELOPMENT AREA 7					RL	RL	II	
TOTAL AREA	14.15 AC							
WETLANDS	0 AC							
DEVELOPABLE AREA	14.15 AC							
UNITS (60' WIDE)	29 DU							
DENSITY	2.05 DU/AC							
TOTAL SF UNITS	61							
ATTACHED SINGLE FAMILY (MAX. 2 STORY)								
	EXISTING FLU	PROPOSED FLA	PHASE					
DEVELOPMENT AREA 8					RM	RM	II	
TOTAL AREA	8.63 AC							
WETLANDS	0 AC							
DEVELOPABLE AREA	8.63 AC							
UNITS (TH)	60 DU							
DENSITY	6.95 DU/AC							
DEVELOPMENT AREA 9					RL	RM	II	
TOTAL AREA	8.48 AC							
WETLANDS	0 AC							
DEVELOPABLE AREA	8.48 AC							
UNITS (TH)	36 DU							
DENSITY	4.25 DU/AC							
DEVELOPMENT AREA 10					RM	RM	II	
TOTAL AREA	15.68 AC							
OPEN SPACE	6.40 AC							
WETLANDS	0 AC							
DEVELOPABLE AREA	15.68 AC							
UNITS (TH)	116 DU							
DENSITY	7.40 DU/AC							
TOTAL TH UNITS	212							
DEVELOPMENT AREA 2 (MAX 3-STORY)					RM	RH	II	
TOTAL AREA	16.53 AC							
WETLANDS	0 AC							
DEVELOPABLE AREA	16.53 AC							
UNITS	240 DU (30/BLDG/3-STORY)							
DENSITY	14.52 DU/AC							
PARKING REQUIRED (1 BLDG = 12/15/3)								
1 BED (1.5 SPA / BED)	144							
2+ BED (2 SPA / BED)	240							
3+ BED (2 SPA / BED)	48							
TOTAL SPACES	432							
PARKING PROVIDED	502							
DEVELOPMENT AREA 3 (MAX 3-STORY)					RH	RH	I	
TOTAL AREA	6.99 AC							
OPEN SPACE	1.27 AC							
WETLANDS	1.2 AC							
DEVELOPABLE AREA	5.79 AC							
UNITS	60 DU (30/BLDG/3-STORY)							
DENSITY	8.58 DU/AC							
PARKING REQUIRED (1 BLDG = 12/15/3)								
1 BED (1.5 SPA / BED)	36							
2+ BED (2 SPA / BED)	60							
3+ BED (2 SPA / BED)	12							
TOTAL SPACES	108							
PARKING PROVIDED	161							
DEVELOPMENT AREA 4 (MAX 4-STORY)					RH	RH	I	
TOTAL AREA	11.72 AC							
OPEN SPACE	3.60 AC							
WETLANDS	0 AC							
DEVELOPABLE AREA	11.72 AC							
UNITS	280 DU (40/BLDG)							
DENSITY	23.89 DU/AC							
PARKING REQUIRED (1 BLDG = 12/15/3)								
1 BED (1.5 SPA / BED)	168							
2+ BED (2 SPA / BED)	280							
3+ BED (2 SPA / BED)	56							
TOTAL SPACES	504							
PARKING PROVIDED	504							
DEVELOPMENT AREA 11 (MAX 3-STORY)					RM	RH	I	
TOTAL AREA	6.81 AC							
OPEN SPACE	2.18 AC							
WETLANDS	0 AC							
DEVELOPABLE AREA	6.81 AC							
UNITS (60' WIDE)	120 DU (30/BLDG)							
DENSITY	17.62 DU/AC							
PARKING REQUIRED (1 BLDG = 12/15/3)								
1 BED (1.5 SPA / BED)	72							
2+ BED (2 SPA / BED)	120							
3+ BED (2 SPA / BED)	24							
TOTAL SPACES	216							
PARKING PROVIDED	270							
DEVELOPMENT AREA 12 (MAX 3-STORY)					RM	RH	I	
TOTAL AREA	1.94 AC							
OPEN SPACE	1.00 AC							
WETLANDS	0 AC							
DEVELOPABLE AREA	1.94 AC							
USE = CLUBHOUSE								
DENSITY	0.00 DU/AC							
PARKING REQUIRED (1 BLDG = 12/15/3)								
1 BED (1.5 SPA / BED)	0							
2+ BED (2 SPA / BED)	0							
3+ BED (2 SPA / BED)	0							
TOTAL SPACES	0							
PARKING PROVIDED	20							
TOTAL MF UNITS	700							
TOTAL PARKING REQUIRED	620							
TOTAL PARKING PROVIDED	1383							
TOTAL SITE SUMMARY								
TOTAL AREA	128.93 AC							
WETLANDS	8.24 AC							
DEVELOPABLE AREA	120.69 AC							
TOTAL UNITS	973 DU							
DENSITY	7.55 DU/AC							
TOTAL STORMWATER	26 AC	20%						